



## Task Order Request #TO25-043 - MOW Training Yard (TTEC)

<b>Status</b>	Open	<b>Assignees</b>	Dean Hansen
<b>Created Date</b>	Aug 21, 2025	<b>Issued Date</b>	Aug 21, 2025

### TASK ORDER IDENTIFICATION

<b>Contract No</b>	23-03811		
<b>Contractor Name ("Contractor")</b>	Stacy and Witbeck, Inc.	<b>Contract Start Date</b>	04/19/24
<b>Account Code(s)</b>	40-3271.63000.3005		

### 1.0 SCOPE OF SERVICES

The contractor's scope letter and price estimate is hereby attached and incorporated into this Task Order [25-630 - MOW Training Yard - Re-Price - Proposal and Scope Letter.pdf](#)

### 2.0 SCHEDULE

<b>The Substantial Completion Date for this Task is</b>	12/31/26	<b>The Final Acceptance Date for this Task is</b>	12/31/26
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### 3.0 PRICING

<b>The pricing agreement for this item is one of the following:</b>	Lump Sum	<b>Invoices will be billed on a monthly basis for completed work to date. The price for this item is in the amount of</b>	\$3,162,902.00
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<b>Provisional Sum Amount (if applicable). Note: Any unused amount of this provisional sum amount will be deducted from the contract upon closeout of the task order.</b>	N/A	<b>Independent Cost Estimate (ICE) link, if applicable</b>	<a href="#">TO25-043 MSP 271 PTO 21C Civil Construction ICE_2024_Final.xlsx</a>
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### 4.0 APPLICABILITY OF FEDERAL CLAUSES

<b>Does this Task Order include federal assistance funds which requires the application of the Federal Clauses appended as</b>	No	<b>If federal assistance funds are anticipated, the UTA Civil Rights group has set a Disadvantaged Business Enterprises (DBE) participation goal</b>	N/A
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Exhibit D to the Contract?

for this Task Order of

UTAH TRANSIT AUTHORITY:

Required Signatures Explanation Project Manager \$0 - 24,999 Legal Review \$10k or greater Dir. of Capital Projects \$25k - 74,999 Chief Service Dev. Ofcr. \$75k - 199,999 Executive Director \$200,000+ Procurement/Contracts (for all)

Signature (Legal) By: Mike Bell Name: Mike Bell Date: 8/22/2025

PM Approval The costs associated with this item have been measured against the standard schedule of rates and the agreed contract pricing, (where applicable) and have been deemed consistent and appropriate for the proposed scope of work.

Signature (Project Manager) By: Dean Hansen Name: Dean Hansen Date: 8/21/2025

Director Approval I have evaluated the content of this task order and the scope of work described in the task ordering agreement and have made the determination that this Task Order is within the scope of work contemplated and described by the contracting parties when they executed the original task ordering agreement.

Signature (Director) By: David Osborn Name: David Osborn Date: 8/21/2025

Signature (Procurement) By: Name: Date:

Signature (Chief Service Development Officer) By: Name: Date:

Signature (Executive Director) By: Jay Fox, Executive Director Date:

COMPANY:

COMPANY: Stacy and Witbeck, Inc.

- RMS Required Signature Explanation N/A Up to \$100K - Josh Lafleur (\$lafleur@modrailsystems.com) \$100K - \$500K - Anthony Ortolani (aortolani@modrailsystems.com) \$500K - \$2.5M - Shon Tulik (stulik@modrailsystems.com) >\$2.5M or Contract Time Extensions - Paul Reiger (prieger@modrailsystems.com)

**Signature  
(Contractor)**

Signed by:  
By: Miguel Gomez  
2644C41CCA1D4F2...  
Name: Miguel Gomez  
Date: 8/22/2025

Date 12-May-2023

**MOW Training Yard – Civil & Track Construction & Procurement**  
**Scope of Work**  
**Contract: 23-03811VW (Infrastructure On-Call Maint.)**  
**UTA Project ID: MSP271**

**Background**

UTA is developing an apprenticeship program for the rail maintenance employees. This program will provide proper training to employees who maintain, operate, inspect, and troubleshoot the UTA rail systems, including the Front Runner Commuter Rail and the TRAX Light Rail systems. UTA is in the process of developing a comprehensive, full-scale training yard that will provide real-life training scenarios on actual equipment utilized on UTA rail systems. The curriculum for the apprenticeship program will utilize the training yard infrastructure and functionality.

**Justification**

This training yard will allow UTA MOW employees to become proficient in the systems they maintain in a safe environment that does not affect revenue trains. Employees will be able to improve their maintenance response times, knowledge base, and overall confidence when troubleshooting mainline system components.

**Scope of Work**

This scope of work is for the **civil procurement and track construction efforts in the training yard only**. This scope does not include construction work related to the rehab of the Transit Technical Education Center (TEC) Facility. The civil and track design for this project will be provided by UTA's internal Capital Development Team. The contractor shall coordinate efforts with all stakeholders per the MOW Training Yard Project Delivery Plan.

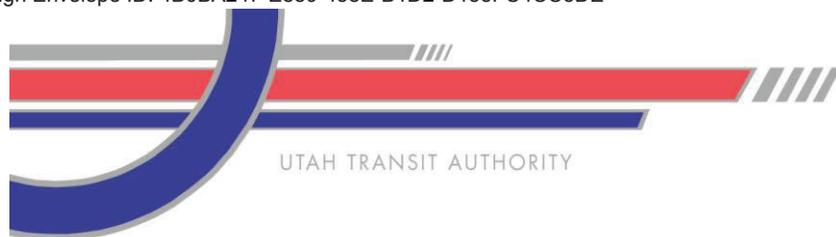
Contractor shall procure, install, and test all infrastructure and material described in the attached design. All materials and hardware shall be built to the same standards as UTA's in-service hardware, unless approved by UTA as an exception. The contractor shall adhere to industry standards and best practices. Live trains will never run on the training facility rails. Hi-Rail equipment will operate on the training facility tracks for training purposes only. Below is a list of key infrastructure to be **procured, constructed, and installed** in this scope. This list is not comprehensive, contractor shall refer to the approved design for complete details

- Contractor to apply and obtain all permitting for work to be done
- Demolition, Clearing, and Earthwork
  - Remove concrete pavement
  - Remove and replace existing waterway
  - Dewatering
- Erosion control: silt fence, straw, seeds, riprap, etc.
- Drainage & Grading

- Site Utilities
  - Drainage pipes, catch basins, and detention ponds with inlets and outlets
  - Relocate power pole and connections to existing building
  - Fire service line to existing TEC facility
- Embedded trackwork with concrete transitions slabs and embedded-to-ballasted transitions
- Ballasted trackwork (install sub-ballast, ballast, ties, and rail)
- Special Trackwork
  - One (1) interlocking with four (4) #10 turnouts
    - Turnouts to be transported to yard from 5300/5400 S project location.
- Bollards at both ends of Track 1 and 3, north end of Track 2
- Graded access road
- Grade crossing (embedded track)
  - Striping
  - 3 gate arms
- Foundations
  - Signal house foundation
  - OCS Pole Foundations (approx. 30)
  - TPSS foundation
  - Pad-mount disconnect switch foundation
  - Main distribution pad
- Ballast pad for four training switch modules
- Conduit trenching, pull boxes, routing, and installation
  - Signals
  - Communications
  - Utility power
  - Lighting
- Concrete-encased duct banks
  - TPSS-to-OCS feeder cables
  - TPSS-to-rail negative return cables
  - Communications, signals, and TPSS
  - Lighting
  - Power distribution
- TPSS transportation and installation (from Beck Yard to the training yard)
- Pedestrian & bike access path

The hourly rates used to complete this task shall be in accordance with the rates as defined in contract 20-03349VW.

Please contact Dean Hansen at telephone number 801-687-3400 or via email at [dhansen@rideuta.com](mailto:dhansen@rideuta.com) if you have any questions.



Sincerely,

**Dean Hansen**

Digitally signed by Dean Hansen  
DN: C=US, E=dhansen@rideuta.com,  
O=Utah Transit Authority, OU="Systems  
Engineering", CN=Dean Hansen  
Date: 2023.11.06 15:40:05-0700

(Signature)

Dean Hansen  
Manager- Systems Engineering  
Utah Transit Authority

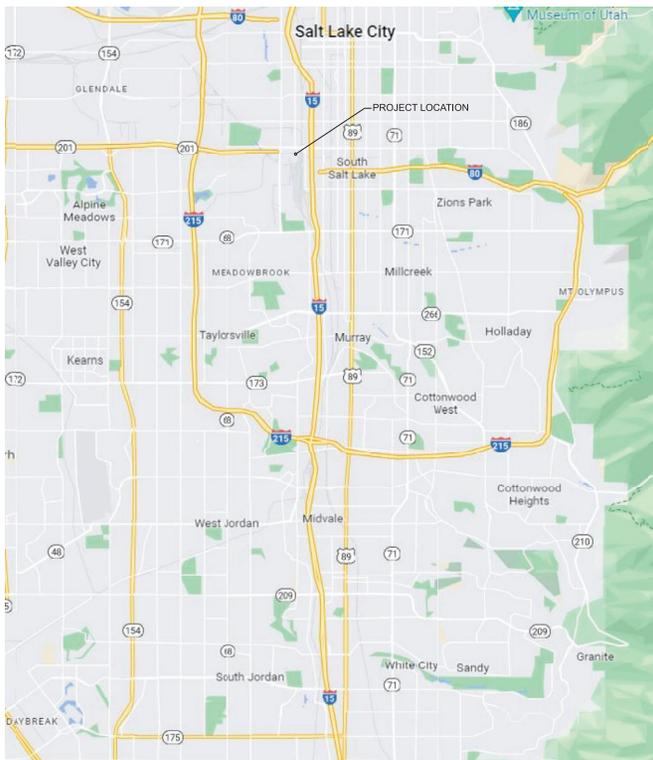


# UTAH TRANSIT AUTHORITY

## RAIL FACILITIES MAINTENANCE OF WAY TRAINING YARD

### GRADING AND DRAINAGE PACKAGE

RELEASED FOR CONSTRUCTION  
MAY 2023



INDEX TO SHEETS	
SHEET NUMBER	DESCRIPTION
1	TITLE SHEET
2	GENERAL NOTES
3	TRACK TYPICAL SECTIONS
4	GRADING AND DRAINAGE SITE PLAN 1 OF 2
5	GRADING AND DRAINAGE SITE PLAN 2 OF 2
6	TRACK ALIGNMENT DATA
7	TRACK PROFILE SHEET 1 OF 2
8	TRACK PROFILE SHEET 2 OF 2
9	GRADE CROSSING DETAIL
10	TRACK SLAB DETAIL SHEET 1 OF 2
11	TRACK SLAB DETAIL SHEET 2 OF 2
12	REFERENCE DRAWING SHEET 1
13	REFERENCE DRAWING SHEET 2
14	REFERENCE DRAWING SHEET 3
15	REFERENCE DRAWING SHEET 4
16	REFERENCE DRAWING SHEET 5
17	DRAINAGE GENERAL NOTES SHEET 1 OF 2
18	DRAINAGE GENERAL NOTES SHEET 2 OF 2
19	DRAINAGE DETAILS SHEET 1 OF 2
20	DRAINAGE DETAILS SHEET 2 OF 2
21	DRAINAGE PROFILES SHEET 1 OF 2
22	DRAINAGE PROFILES SHEET 2 OF 2
23	DRAINAGE POND PLAN AND PROFILE SHEET 1 OF 2
24	DRAINAGE POND PLAN AND PROFILE SHEET 2 OF 2

△	10/13/22	60% DESIGN
△	1/13/22	100% DESIGN
△	5/08/23	RFC
△		
△		
△		
REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_



Designed By:	BH
Drawn By:	BH
Checked By:	GT
Approved By:	JS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD  
SOUTH SALT LAKE, UT

TITLE SHEET

Scale:	AS SHOWN
CADD Filename:	TITLE SHEET
Submital Date:	05/08/23
UTA Contract No.:	
Drawing No.:	Sheet No.:
T-01	1

**COMPLIANCE**

- ALL WORK TO CONFORM TO UTA, SOUTH SALT LAKE, AND APWA STANDARDS, SPECIFICATIONS, AND REQUIREMENTS.
- ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH THESE CONTRACT DOCUMENTS AND THE MOST RECENT, ADOPTED EDITIONS OF THE FOLLOWING: INTERNATIONAL BUILDING CODE (IBC), THE INTERNATIONAL PLUMBING CODE, STATE DRINKING WATER REGULATIONS, APWA MANUAL OF STANDARD PANS AND SPECIFICATIONS.
- ALL CONSTRUCTION SHALL BE AS SHOWN ON THESE PLANS. ANY REVISIONS MUST HAVE PRIOR WRITTEN APPROVAL.

**PERMITTING AND INSPECTIONS**

- PRIOR TO STARTING CONSTRUCTION, THE CONTRACTOR SHALL PAY FOR AND ENSURE ALL REQUIRED PERMITS AND APPROVALS HAVE BEEN OBTAINED, NO CONSTRUCTION OR FABRICATION SHALL BEGIN UNTIL THE CONTRACTOR HAS RECEIVED THOROUGHLY REVIEWED PLANS AND OTHER DOCUMENTS APPROVED BY ALL OF THE PERMITTING AUTHORITIES, UTA WILL PAY FOR IMPACT FEES.
- CONTRACTOR IS RESPONSIBLE FOR SCHEDULING AND NOTIFYING ENGINEER OR INSPECTING AUTHORITY 48 HOURS IN ADVANCE OF COVERING UP ANY PHASE OF CONSTRUCTION REQUIRING OBSERVATION.
- ANY WORK IN THE PUBLIC RIGHT-OF-WAY WILL REQUIRE PERMITS FROM THE APPROPRIATE CITY, COUNTY, OR STATE AGENCY CONTROLLING THE ROAD AND WITH APPROPRIATE INSPECTIONS.

**COORDINATION AND VERIFICATION**

- ALL DIMENSIONS, GRADES & UTILITY DESIGNS SHOWN ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO CONSTRUCTION, CONTRACTOR SHALL NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO PROCEEDING WITH CONSTRUCTION FOR NECESSARY PLAN OR GRADE CHANGES, NO EXTRA COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO DIMENSIONS OR GRADES SHOWN INCORRECTLY ON THESE PLANS, IF NOT VERIFIED AND NOTIFICATION OF CONFLICTS HAVE NOT BEEN BROUGHT TO THE ATTENTION OF THE ENGINEER.
- CONTRACTOR MUST VERIFY ALL EXISTING CONDITIONS BEFORE BIDDING AND BRING UP ANY QUESTIONS BEFOREHAND, NO ALLOWANCE WILL BE MADE FOR DISCREPANCIES OR OMISSIONS THAT CAN BE EASILY OBSERVED.
- CONTRACTOR TO COORDINATE WITH ALL OTHER DISCIPLINES, INCLUDING BUT NOT LIMITED TO: LANDSCAPE PLANS, SITE ELECTRICAL, SITE LIGHTING, AND ELECTRICAL SERVICE TO THE BUILDING(S), ETC., NOT SHOWN ON CIVIL PLANS.

**SAFETY AND PROTECTION**

- CONTRACTOR IS SOLELY RESPONSIBLE FOR THE MEANS AND METHODS OF CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR THE SAFETY OF THE PROJECT AND SHALL MEET ALL OSHA REQUIREMENTS.
- CONTRACTOR IS RESPONSIBLE FOR CONFORMING TO LOCAL AND FEDERAL CODES GOVERNING SHORING AND BRACING OF EXCAVATIONS AND TRENCHES, AND FOR THE PROTECTION OF WORKERS AND PUBLIC.
- CONTRACTOR SHALL TAKE ALL MEASURES NECESSARY TO PROTECT ALL EXISTING PUBLIC AND PRIVATE PROPERTY, ROADWAYS, AND UTILITY IMPROVEMENTS, DAMAGE TO EXISTING IMPROVEMENTS CAUSED BY THE CONTRACTOR MUST BE REPAIRED BY THE CONTRACTOR AT THEIR EXPENSE TO THE SATISFACTION OF THE OWNER OF SAID IMPROVEMENTS.
- CONTRACTOR IS REQUIRED TO KEEP ALL CONSTRUCTION ACTIVITIES WITHIN THE APPROVED PROJECT LIMITS. THIS INCLUDES, BUT IS NOT LIMITED TO, VEHICLE AND EQUIPMENT STAGING, MATERIAL STORAGE, AND LIMITS OF TRENCH EXCAVATION.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO OBTAIN PERMISSIONS AND/OR EASEMENTS FROM THE APPROPRIATE GOVERNMENT AGENCY AND/OR INDIVIDUAL PROPERTY OWNER(S) FOR WORK OR STAGING OUTSIDE OF THE PROJECT LIMITS.
- CONTRACTOR SHALL PROVIDE BARRICADES, SIGNS, FLASHERS, OTHER EQUIPMENT AND FLAG PERSONS NECESSARY TO INSURE THE SAFETY OF WORKERS AND VISITORS. ALL CONSTRUCTION SIGNING, BARRICADING, AND TRAFFIC DELINEATION SHALL CONFORM TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", LATEST EDITION.
- CONTRACTOR SHALL COMPLY WITH LOCAL NOISE ORDINANCE STANDARDS.
- CONTRACTOR IS RESPONSIBLE FOR DUST CONTROL ACCORDING TO GOVERNING AGENCY STANDARDS.
- CONTRACTOR SHALL TAKE ALL NECESSARY AND PROPER PRECAUTIONS TO PROTECT ADJACENT PROPERTIES FROM ANY AND ALL DAMAGE THAT MAY OCCUR FROM STORM WATER RUNOFF AND/OR DEPOSITION OF DEBRIS RESULTING FROM ANY AND ALL WORK IN CONNECTION WITH CONSTRUCTION, AND SUBMIT A STORM WATER POLLUTION PREVENTION PLAN, IF REQUIRED.
- WORK IN PUBLIC STREETS, ONCE BEGUN, SHALL BE PROSECUTED TO COMPLETION WITHOUT DELAY AS TO PROVIDE MINIMUM INCONVENIENCE TO ADJACENT PROPERTY OWNERS AND TO THE TRAVELING PUBLIC.
- CONTRACTOR SHALL PROVIDE ALL NECESSARY HORIZONTAL AND VERTICAL TRANSITIONS BETWEEN NEW CONSTRUCTION AND EXISTING SURFACES TO PROVIDE FOR PROPER DRAINAGE AND FOR INGRESS AND EGRESS TO NEW CONSTRUCTION.

- NATURAL VEGETATION AND SOIL COVER SHALL NOT BE DISTURBED PRIOR TO ACTUAL CONSTRUCTION OF A REQUIRED FACILITY OR IMPROVEMENT. MASS CLEARING OF THE SITE IN ANTICIPATION OF CONSTRUCTION SHALL BE AVOIDED, CONSTRUCTION TRAFFIC SHALL BE LIMITED TO ONE APPROACH TO THE SITE, UNLESS OTHERWISE DIRECTED BY THE PROJECT SWPPP, THE APPROACH SHALL BE DESIGNATED BY THE OWNER OR GOVERNING AGENCY.
- THE CONTRACTOR SHALL TAKE REASONABLE MEASURE TO PROTECT EXISTING IMPROVEMENTS FROM DAMAGE AND ALL SUCH IMPROVEMENTS DAMAGED BY THE CONTRACTOR'S OPERATION SHALL BE REPAIRED OR RECONSTRUCTED TO THE ENGINEER/OWNER'S SATISFACTION AT THE EXPENSE OF THE CONTRACTOR. THE CONTRACTOR SHALL REVIEW AND COMPLY WITH THE UTA SAFETY MANUAL.

**MATERIALS**

- SITE CONCRETE SHALL BE A MINIMUM 6.5 BAG MIX, 4000 P.S.I. @ 28 DAYS, 4" MAXIMUM SLUMP WITH 5% TO 7% AIR ENTRAINMENT, UNLESS SPECIFIED OTHERWISE, SEE SPECIFICATIONS.
- SLABS-ON-GRADE WILL BE TYPICALLY SCORED (1/4 THE DEPTH) AT INTERVALS NOT TO EXCEED THEIR WIDTH OR 12 TIMES THEIR DEPTH, WHICHEVER IS LESS. SCORING WILL BE PLACED TO PREVENT RANDOM CRACKING, FULL DEPTH EXPANSION JOINTS WILL BE PLACED AGAINST ANY OBJECT DEEMED TO BE FIXED, CHANGES IN DIRECTION AND AT EQUAL INTERVALS NOT TO EXCEED 50 FEET.
- CONCRETE WATERWAYS, CURB WALLS, MOWSTRIPS, CURB AND GUTTER, ETC., WILL TYPICALLY BE SCORED (1/4 THE DEPTH) AT INTERVALS NOT TO EXCEED 10 FEET AND HAVE FULL DEPTH EXPANSION JOINTS AT EQUAL SPACING NOT TO EXCEED 50 FEET.
- UNLESS OTHERWISE NOTED, ALL SLABS-ON-GRADE WILL HAVE A MINIMUM 8" TURNED-DOWN EDGE TO HELP CONTROL FROST HEAVE.
- UNLESS OTHERWISE NOTED, A 4" LAYER OF PORTLAND CEMENT CONCRETE (9" FOR BUS TRAFFIC AREAS) SHALL BE PLACED OVER A MINIMUM OF 4" UNTREATED ROAD BASE OVER A WELL COMPACTED (95%) SUBGRADE. SEE PROJECT TYPICAL SECTIONS.
- ALL EXPOSED SURFACES WILL HAVE A TEXTURED FINISH, RUBBED OR BROOMED, ANY "PLASTERING" OF NEW CONCRETE WILL BE DONE WHILE IT IS STILL "GREEN".
- ALL JOINTS (CONTROL, CONSTRUCTION OR EXPANSION JOINTS, ETC.) WILL BE SEALED WITH A ONE PART POLYURETHANE SEALANT (SEE SPECIFICATIONS).

- ASPHALTIC CONCRETE PAVEMENT SHALL BE A MINIMUM 3" OVER 8" OF COMPACTED (95%) ROAD BASE OVER 8" GRANULAR BORROW OVER PROPERLY PREPARED AND COMPACTED (95%) SUBGRADE FOR CAR TRAFFIC, AND A MINIMUM 7" ASPHALTIC CONCRETE OVER 9" OF COMPACTED (95%) ROAD BASE OVER 10" OF GRANULAR BORROW OVER A TENSAR TX 160 GEOGRID (OR AN APPROVED EQUIVALENT) OVER PROPERLY PREPARED AND COMPACTED (95%) SUBGRADE FOR BUS TRAFFIC, UNLESS NOTED OTHERWISE, SEE SPECIFICATIONS.

- ASPHALT COMPACTION SHALL BE A MINIMUM 96% (MARSHALL DESIGN).
- SURFACE COARSE SHALL BE 1/2" MINUS, MIX DESIGN TO BE SUBMITTED FOR APPROVAL AT LEAST TWO WEEKS PRIOR TO ANTICIPATED PAVING SCHEDULE.
- AC PAVEMENT TO BE A 1/2" ABOVE LIP OF ALL GUTTER (EXCEPT SPILL AND COLLAR CURB) AFTER COMPACTION.
- THICKNESSES OVER 3" WILL BE LAID IN TWO LIFTS WITH THE FIRST LIFT BEING AN APPROVED 3/4" MINUS DESIGN.

**UTILITIES**

- THE LOCATIONS OF UNDERGROUND FACILITIES SHOWN ON THESE PLANS ARE BASED ON FIELD SURVEYS AND LOCAL UTILITY COMPANY RECORDS, IT SHALL BE THE CONTRACTOR'S FULL RESPONSIBILITY TO CONTACT THE VARIOUS UTILITY COMPANIES EITHER DIRECT OR THROUGH BLUE STAKE TO LOCATE THEIR FACILITIES PRIOR TO STARTING CONSTRUCTION.
- CONTRACTOR TO VERIFY BY POT-HOLING BOTH THE VERTICAL AND HORIZONTAL LOCATION OF ALL EXISTING UTILITIES PRIOR TO INSTALLING ANY NEW LINES, NO ADDITIONAL COMPENSATION SHALL BE PAID TO THE CONTRACTOR FOR DAMAGE AND REPAIR TO THESE FACILITIES CAUSED BY THEIR WORK FORCE.
- CONTRACTOR MUST START AT LOW END OF ALL NEW GRAVITY UTILITY LINES, NO EXTRA COMPENSATION IS TO BE PAID TO THE CONTRACTOR FOR WORK HAVING TO BE REDONE DUE TO FAILURE TO COMPLY WITH THESE REQUIREMENTS.
- CONTRACTOR IS TO VERIFY LOCATION, DEPTH, SIZE, TYPE, AND OUTSIDE DIAMETERS OF UTILITIES IN THE FIELD BY POT-HOLING A MINIMUM OF 300 FEET AHEAD, PIPELINE CONSTRUCTION TO AVOID CONFLICTS WITH DESIGNED PIPELINE GRADE AND ALIGNMENT. EXISTING UTILITY INFORMATION SHOWN ON PLANS OR OBTAINED FROM UTILITY COMPANIES OR BLUE STAKED MUST BE ASSUMED AS APPROXIMATE, REQUIRING FIELD VERIFICATION.
- FIRE SERVICE LINES TO BE CONSTRUCTED IN ACCORDANCE WITH LOCAL GOVERNING MUNICIPALITY STANDARDS AND SPECIFICATIONS.
- STORM DRAIN TO BE CONSTRUCTED IN ACCORDANCE WITH THE GOVERNING MUNICIPALITY STANDARDS AND SPECIFICATIONS.
- ALL STORM DRAIN AND IRRIGATION CONDUITS SHALL BE INSTALLED WITH WATER TIGHT JOINTS AND CONNECTIONS.
- ALL STORM DRAIN PIPE PENETRATIONS INTO BOXES SHALL BE CONSTRUCTED WITH WATER TIGHT SEALS ON THE OUTSIDE AND GROUTED SMOOTH WITH A NON-SHRINK GROUT ON THE INSIDE, CONDUITS SHALL BE CUT OFF FLUSH WITH THE INSIDE OF THE BOX.

- NO CHANGE IN THE DESIGN OF UTILITIES AS SHOWN WILL BE MADE BY THE CONTRACTOR WITHOUT THE WRITTEN APPROVAL OF THE GOVERNING MUNICIPALITY, OR OTHER AUTHORITY HAVING JURISDICTION OVER THAT UTILITY.
- ALL STORM DRAIN CONDUITS AND BOXES SHALL BE CLEAN AND FREE OF ROCKS, DIRT, AND CONSTRUCTION DEBRIS PRIOR TO FINAL INSPECTION.
- CONTRACTOR IS RESPONSIBLE FOR THE COST AND SCHEDULING OF REQUIRED UTILITIES TO THE PROJECT.

**AMERICANS WITH DISABILITIES ACT**

- PEDESTRIAN / ADA ROUTES SHALL MEET THE FOLLOWING SPECIFICATIONS:  
 - ROUTES SHALL HAVE A 2.00% (1:50) MAXIMUM CROSS SLOPE.  
 - ROUTES SHALL HAVE A 5.00% (1:20) MAXIMUM SLOPE.  
 - RAMPS SHALL HAVE A 8.33% (1:12) MAXIMUM SLOPE.
- ADA PARKING STALLS AND ADJACENT ROUTES SHALL HAVE A 2.00% MAXIMUM SURFACE SLOPE IN ANY DIRECTION, UNLESS OTHERWISE NOTED.
- THE CONTRACTOR SHALL ADHERE TO THE ABOVE SPECIFICATIONS, IN THE EVENT OF A DISCREPANCY IN THE CONSTRUCTION DOCUMENTS, THE CONTRACTOR SHALL NOTIFY THE ARCHITECT/ENGINEER PRIOR TO ANY CONSTRUCTION.

**GRADING / SOILS**

- SITE GRADING SHALL BE PERFORMED IN ACCORDANCE WITH THESE PLANS AND SPECIFICATIONS.
- ALL EXCAVATED OR FILLED AREAS SHALL BE COMPACTED TO 95% OF MODIFIED PROCTOR MAXIMUM DENSITY PER ASTM TEST D-1557, EXCEPT UNDER BUILDING FOUNDATIONS WHERE IT SHALL BE 98% MIN. OF MAXIMUM DENSITY, MOISTURE CONTENT AT TIME OF PLACEMENT SHALL NOT EXCEED 2% ABOVE NOR 3% BELOW OPTIMUM.
- SITE CLEARING SHALL INCLUDE THE LOCATING AND REMOVAL OF ALL UNDERGROUND TANKS, PIPES, VALVES, ETC., OR FILLING WITH FLOWABLE FILL AS DETERMINED BY THE ENGINEER.
- ALL EXISTING VALVES, MANHOLES, ETC. SHALL BE RAISED OR LOWERED TO GRADE AS REQUIRED.

**GENERAL CONSTRUCTION NOTES**

- ALL CONSTRUCTION SHALL CONFORM TO THE STANDARDS AND SPECIFICATIONS OF SOUTH SALT LAKE, IN ADDITION, ALL WORK SHALL COMPLY WITH THE PROJECT DRAWINGS, PROJECT SPECIFICATIONS, IF CONFLICTS ARE NOTED, CONTRACTOR SHALL CONTACT ENGINEER PRIOR TO START OF CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE WITH ALL GOVERNING AGENCIES FOR ALL PERMITS AND BONDS REQUIRED FOR CONSTRUCTION, CONTRACTOR SHALL ALSO BE RESPONSIBLE INSPECTION AND REVIEW OF WORK.
- CONTRACTOR IS RESPONSIBLE FOR ALL PROJECT SAFETY, INCLUDING BUT NOT LIMITED TO TRENCHING AND SHORING, TRAFFIC CONTROL, AND SECURITY.
- CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN UP-TO-DATE "AS BUILT" DRAWINGS THROUGHOUT THE PROJECT, THESE DRAWINGS SHALL BE PROVIDED TO THE OWNER UPON COMPLETION OF THE PROJECT.
- CONTRACTOR IS CAUTIONED THAT THE LOCATION AND ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE DRAWINGS IS BASED ON RECORDS OF UTILITIES COMPANIES AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD, THE INFORMATION IS NOT TO BE RELIED ON AS EXACT OR COMPLETE, CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING ALL UTILITIES AND SERVICE LATERALS AND FOR REPAIRING ALL DAMAGE THAT OCCURS TO THE UTILITY DURING CONSTRUCTION.
- CONTRACTOR SHALL TAKE THE NECESSARY MEASURES TO PROTECT ALL FACILITIES (I.E, PIPES STRUCTURES, ETC.) DURING CONSTRUCTION UNTIL THE DESIGN GRADE AND COVER HAS BEEN ATTAINED AND WORK HAS BEEN ACCEPTED.
- CONTRACTOR SHALL VERIFY ALL EXISTING PROJECT MONUMENTS AND CONTROL AND NOTIFY ENGINEER OF ANY PROBLEMS PRIOR TO START OF CONSTRUCTION, CONTRACTOR SHALL MAINTAIN OR RESTORE ALL MONUMENTS AND MONUMENT REFERENCE MARKS WITHIN THE PROJECT SITE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR DUST AND EROSION CONTROL 24 HOURS PER DAY UNTIL VEGETATION IS ESTABLISHED, AND MAINTAIN UNIMPEDED ACCESS ROAD MAINTENANCE IN ACCORDANCE WITH CITY STANDARDS.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TEMPORARY TRAFFIC CONTROL DURING THE CONSTRUCTION OF THE PROJECT, UNTIL WORK IS APPROVED AND ACCEPTED BY OWNER.
- ALL TRAFFIC CONTROL (I.E, SIGNAGE, STRIPING, AND PAVEMENT MARKINGS) SHALL CONFORM TO THE MOST CURRENT EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- CONTRACTOR WILL BE RESPONSIBLE FOR THE COST AND SCHEDULING OF ALL QUALITY CONTROL INSPECTIONS, TESTING, AND SAMPLING AS OUTLINED IN THE PROJECT SPECIFICATIONS.
- UTA WILL PROVIDE QUALITY ASSURANCE FOR PROJECT.

△	10/13/22	60% DESIGN
△	1/13/22	100% DESIGN
△	5/08/23	RFC
△		
△		
△		
REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

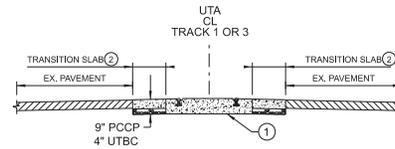


Designed By:	BH
Drawn By:	BH
Checked By:	GT
Approved By:	JS

UTA FACILITIES  
 M.O.W. TRAINING YARD  
 823 W DAVIS RD  
 SOUTH SALT LAKE, UT

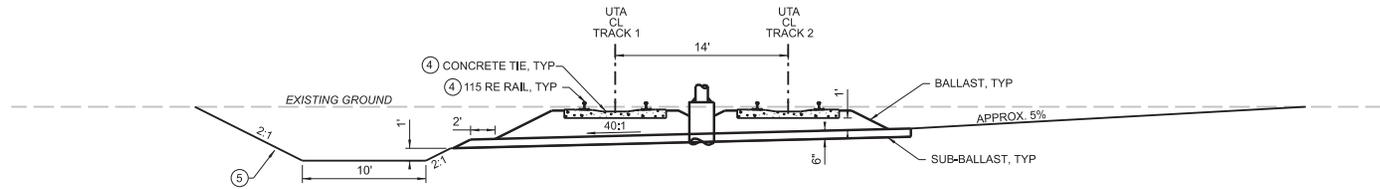
GENERAL NOTES

Scale:	AS SHOWN
CADD Filename:	TITLE SHEET
Submital Date:	05/08/23
UTA Contract No.:	
Drawing No.:	GN-01
Sheet No.:	2



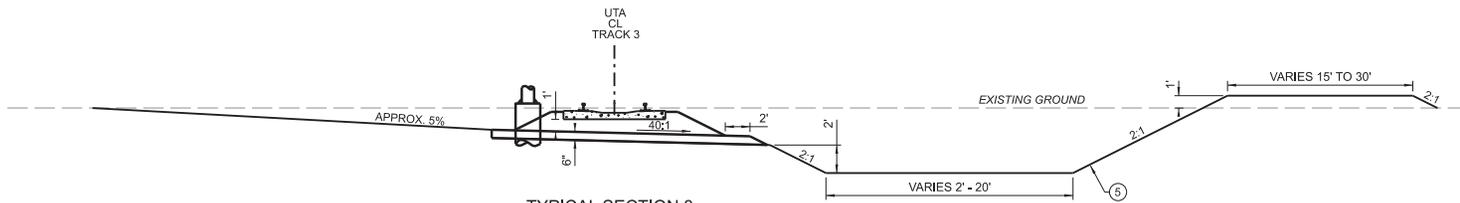
TYPICAL SECTION 1

STA 100+00 TO STA 103+00  
STA 300+00 TO STA 300+40



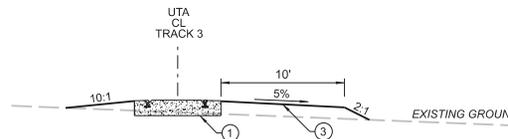
TYPICAL SECTION 2

STA 103+00 TO STA 107+62



TYPICAL SECTION 3

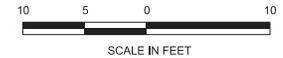
STA 300+40 TO STA 304+32



TYPICAL SECTION 4

STA 304+32 TO STA 307+47

- KEY NOTES:
- ① TRACK SLAB, SEE REFERENCE SHEETS.
  - ② WIDTH AND SLOPE VARIES, SEE DETAIL SHEETS.
  - ③ UTBC ACCESS ROAD
  - ④ NEW OR SALVAGED (COORDINATE WITH UTA)
  - ⑤ SEE DRAINAGE POND PLAN & PROFILE SHEETS



△	10/13/22	60% DESIGN
△	1/13/22	100% DESIGN
△	5/08/23	RFC
△		
△		
△		
REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_



Designed By:	BH, TJ
Drawn By:	BH, TJ
Checked By:	GT, RS
Approved By:	JS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

TRACK TYPICAL SECTIONS  
SHEET 1 OF 1

Scale:	AS SHOWN
CADD Filename:	TRACK TYPICAL SECTIONS
Submitted Date:	05/08/23
UTA Contract No.:	
Drawing No.:	TTS-01
Sheet No.:	3

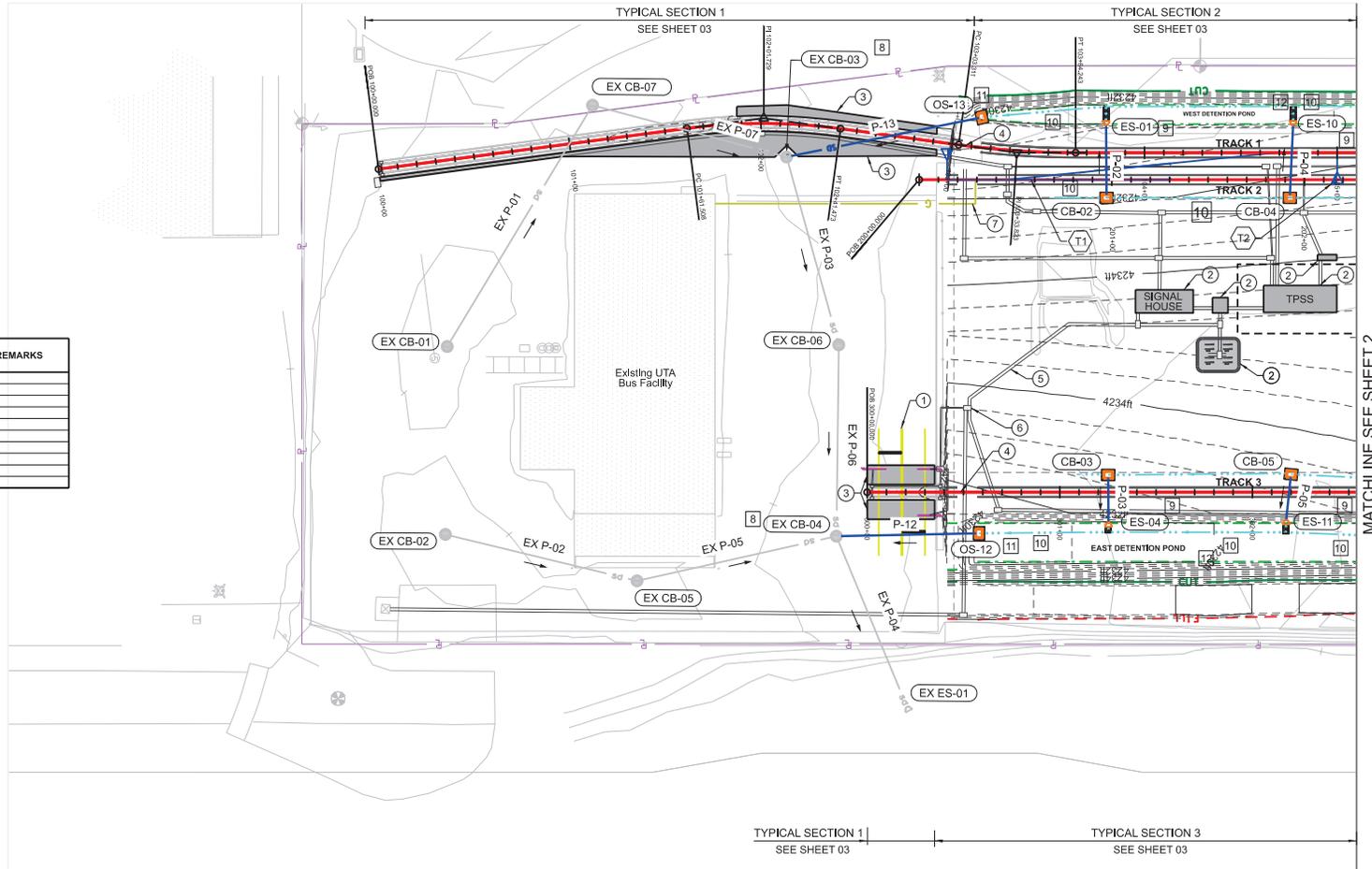
DRAINAGE STRUCTURE ID	LAYOUT (SEE DRDT-01)	ALIGNMENT	STATION	OFFSET	REMARKS
CB-02	D	TRACK 1	103+80.24	23.40	RT
CB-03	D	TRACK 1	103+81.10	167.20	RT
CB-04	D	TRACK 1	104+75.41	23.42	RT
CB-05	D	TRACK 1	104+75.99	166.84	RT
ES-01	C	TRACK 1	103+79.88	14.31	LT
ES-04	C	TRACK 1	103+81.26	191.92	RT
ES-10	C	TRACK 1	104+77.96	14.31	LT
ES-11	C	TRACK 1	104+73.48	190.77	RT
OS-12	D	TRACK 1	103+29.41	199.42	RT
OS-13	D	TRACK 1	103+13.62	15.67	LT

KEY NOTES:

- ① AT-GRADE CROSSING, SEE SHEET 09.
- ② SIGNAL EQUIPMENT (BY OTHERS).
- ③ TRANSITION SLAB, SEE SHEETS 10 & 11.
- ④ PAVED TRACK TRANSITION SECTION, SEE REFERENCE SHEETS.
- ⑤ CONDUIT (TYP.).
- ⑥ JUNCTION BOX (TYP.).
- ⑦ GAS CONNECTION TO SWITCH HEATER (BY OTHERS).

DRAINAGE PLAN & PROFILE NOTES:

- ⑧ CONNECT PROPOSED PIPE TO EXISTING STRUCTURE, CORE AND GROUT A WATERTIGHT CONNECTION, SEE DETAIL A ON SHEET DRDT-01
- ⑨ SEE SHEET DRDT-01 FOR RIPRAP DETAILS.
- ⑩ SEE SHEETS GR-01 AND GR-02 FOR DITCH DETAILS.
- ⑪ SEE SHEET DRDT-02 FOR POND OUTLET STRUCTURE DETAILS.
- ⑫ SEE SHEETS GR-01 AND GR-02 FOR POND GRADING DETAILS.



TURNOUT DATA			
ID NO.	ACTION	PT_SW STA.	P/TO STA.
T1	INSTALL SALVAGED #10 LH TO	200+14.52	200+45.84
T2	INSTALL SALVAGED #10 LH TO	105+00.00	104+68.58

Scale In Feet  
10 5 0 10 20 30 40 50 60

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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REV	DATE	Description

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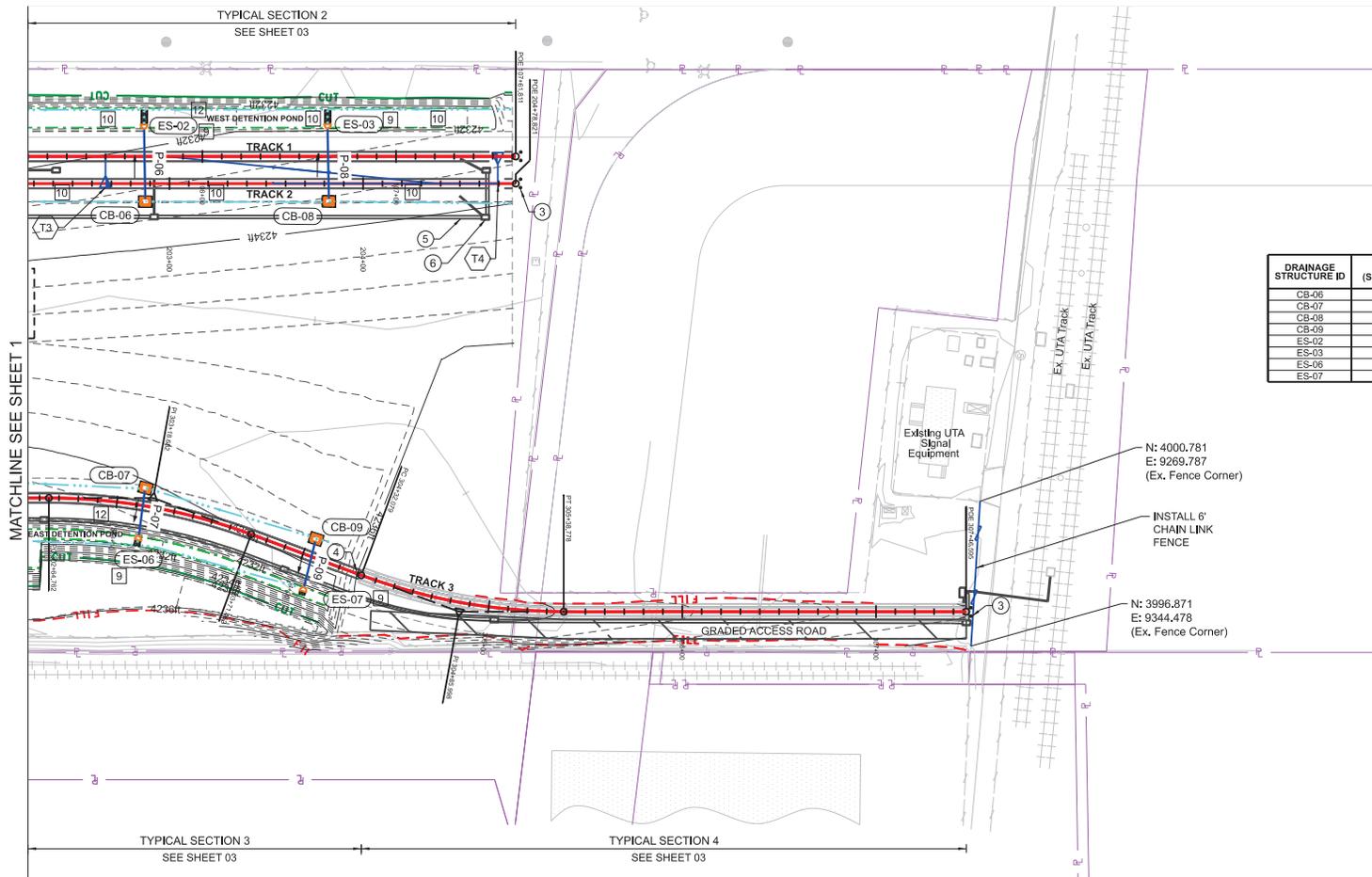
Approved By: \_\_\_\_\_



Designed By: BH, TJ  
 Drawn By: BH, TJ  
 Checked By: GT, RS  
 Approved By: JS

UTA FACILITIES  
 M.O.W. TRAINING YARD  
 823 W DAVIS RD, SOUTH SALT LAKE, UT  
 GRADING AND DRAINAGE SITE PLAN  
 SHEET 1 OF 2

Scale: AS SHOWN  
 CADD Filename: GR-DR PLAN SHEET 01  
 Submit Date: 05/08/23  
 UTA Contract No.:  
 Drawing No.: GRDR-01  
 Sheet No.: 4



DRAINAGE STRUCTURE ID	LAYOUT (SEE DRDT-01)	ALIGNMENT	STATION	OFFSET	REMARKS
CB-06	D	TRACK 1	105+70.48	23.28	RT
CB-07	D	TRACK 1	105+70.65	171.01	RT
CB-08	D	TRACK 1	106+45.29	23.29	RT
CB-09	D	TRACK 1	106+58.65	197.28	RT
ES-02	C	TRACK 1	105+69.89	14.32	LT
ES-03	C	TRACK 1	106+64.63	14.47	LT
ES-06	C	TRACK 1	105+67.18	195.71	RT
ES-07	C	TRACK 1	106+52.59	222.09	RT

- KEY NOTES:
- ① AT-GRADE CROSSING. SEE SHEET 09.
  - ② SIGNAL EQUIPMENT (BY OTHERS).
  - ③ BOLLARDS (TYP), YELLOW, 4' HIGH, 5' SPACING.
  - ④ PAVED TRACK TRANSITION SECTION. SEE REFERENCE SHEETS.
  - ⑤ CONDUIT (TYP, BY OTHERS).
  - ⑥ JUNCTION BOX (TYP, BY OTHERS).

- DRAINAGE PLAN & PROFILE NOTES:
- ⑧ CONNECT PROPOSED PIPE TO EXISTING STRUCTURE, CORE AND GROUT A WATERTIGHT CONNECTION. SEE DETAIL A ON SHEET DRDT-01
  - ⑨ SEE SHEET DRDT-01 FOR RIPRAP DETAILS.
  - ⑩ SEE SHEETS GR-01 AND GR-02 FOR DITCH DETAILS.
  - ⑪ SEE SHEET DRDT-02 FOR POND OUTLET STRUCTURE DETAILS.
  - ⑫ SEE SHEETS GR-01 AND GR-02 FOR POND GRADING DETAILS.

TURNOUT DATA			
ID NO.	ACTION	PT_SW STA.	PITO STA.
T3	INSTALL SALVAGED #10 RH TO	204+69.50	204+38.08
T4	INSTALL SALVAGED #10 RH TO	105+50.00	105+81.42



REV	DATE	Description
△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
△		
△		
△		

Submitted By: \_\_\_\_\_



Designed By: BH, TJ  
 Drawn By: BH, TJ  
 Checked By: GT, RS  
 Approved By: JS

UTA FACILITIES  
 M.O.W. TRAINING YARD  
 823 W DAVIS RD, SOUTH SALT LAKE, UT  
 GRADING AND DRAINAGE SITE PLAN  
 SHEET 2 OF 2

Scale:	AS SHOWN
CADD Filename:	GR-DR_PLAN_SHEET 02
Submitted Date:	AS SHOWN
UTA Contract No.:	
Drawing No.:	GRDR-02
Sheet No.:	5

Element	Point Type	Station	Nerthing	Eating	Radius	Length	Delta	Rotation Direction
<b>AlignmentName:</b>		<b>Track 1</b>						
Tangent	START	100+00.00	3300.49	9110.19				
Tangent	PC	101+61.51	3160.40	9067.50				
Arc	PC	101+61.51	3160.40	9067.50				
Arc	HPI	102+01.73	3199.42	9061.85	300	79.97	15°16'19.96"	Right
Arc	CC		3202.13	9364.53				
Arc	PT	102+41.47	3239.12	9066.89				
Tangent	PT	102+41.47	3239.12	9066.89				
Tangent	PC	103+03.31	3301.47	9064.64				
Arc	PC	103+03.31	3301.47	9064.64				
Arc	HPI	103+33.82	3331.34	9058.46	450	60.93	07°45'29.15"	Left
Arc	CC		3357.46	8648.19				
Arc	PT	103+64.24	3361.85	9058.17				
Tangent	PT	103+64.24	3361.85	9058.17				
Tangent	END	107+61.81	3759.40	9054.28				
<b>AlignmentName:</b>		<b>Track 2</b>						
Tangent	START	200+00.00	3280.14	9112.96				
Tangent	END	204+78.82	3759.44	9108.28				
<b>AlignmentName:</b>		<b>Track 3</b>						
Tangent	START	300+00.00	3255.34	9275.37				
Tangent	PC	302+64.76	3320.49	9272.79				
Arc	PC	302+64.76	3320.49	9272.79				
Arc	HPI	303+18.68	3374.41	9272.26	300	106.70	20°22'40.84"	Right
Arc	CC		3323.42	9572.77				
Arc	PT	303+71.46	3324.13	9250.54				
Tangent	PT	303+71.46	3324.13	9250.54				
Tangent	PC	304+32.08	3381.16	9311.09				
Arc	PC	304+32.08	3381.16	9311.09				
Arc	HPI	304+86.00	3732.49	9328.37	300	106.70	20°22'40.84"	Left
Arc	CC		3783.47	9028.86				
Arc	PT	305+38.78	3786.40	9328.84				
Tangent	PT	305+38.78	3786.40	9328.84				
Tangent	END	307+46.59	3994.11	9326.81				

REV	DATE	Description
△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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Submitted By: \_\_\_\_\_

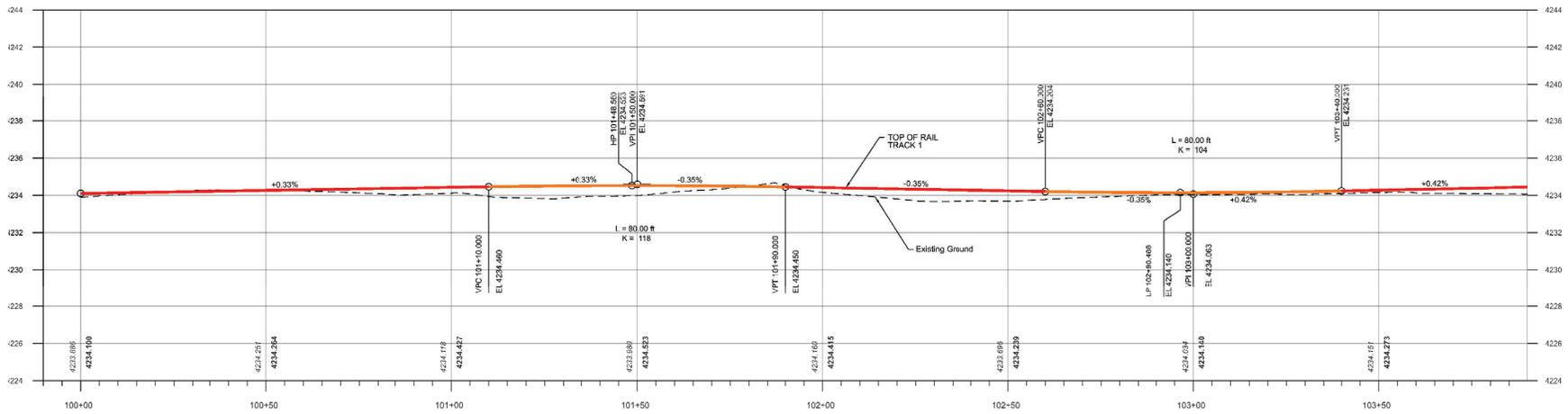


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Drawn By:	BH
Checked By:	GT
Approved By:	JS

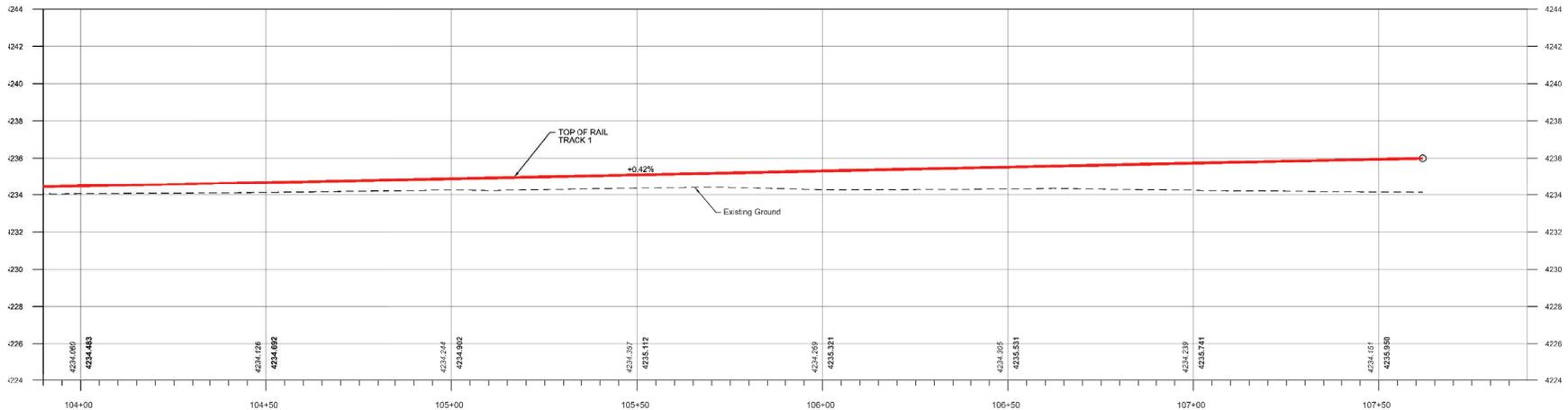
UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

TRACK ALIGNMENT DATA  
SHEET 1 OF 1

Scale:	AS SHOWN
CADD Filename:	TRACK ALIGNMENT DATA
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	TAD-01
Sheet No.:	6



TRACK 1 PROFILE



TRACK 1 PROFILE

NOTES:  
1. CONSTRUCT TRACK 2 PROFILE EQUAL AND PERPENDICULAR TO TRACK 1 PROFILE.

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

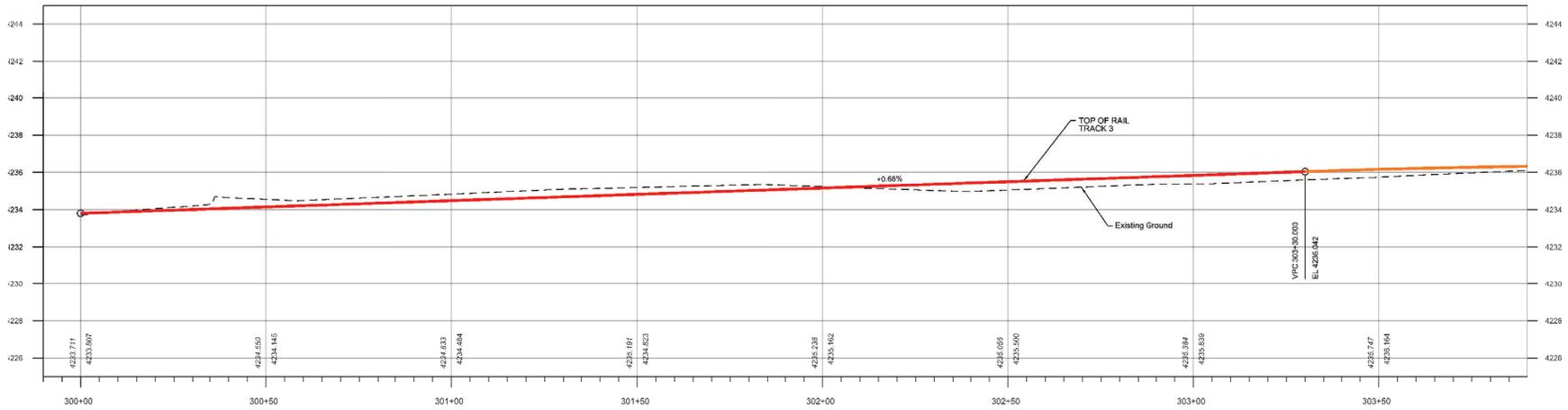


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Drawn By:	BH
Checked By:	GT
Approved By:	JS

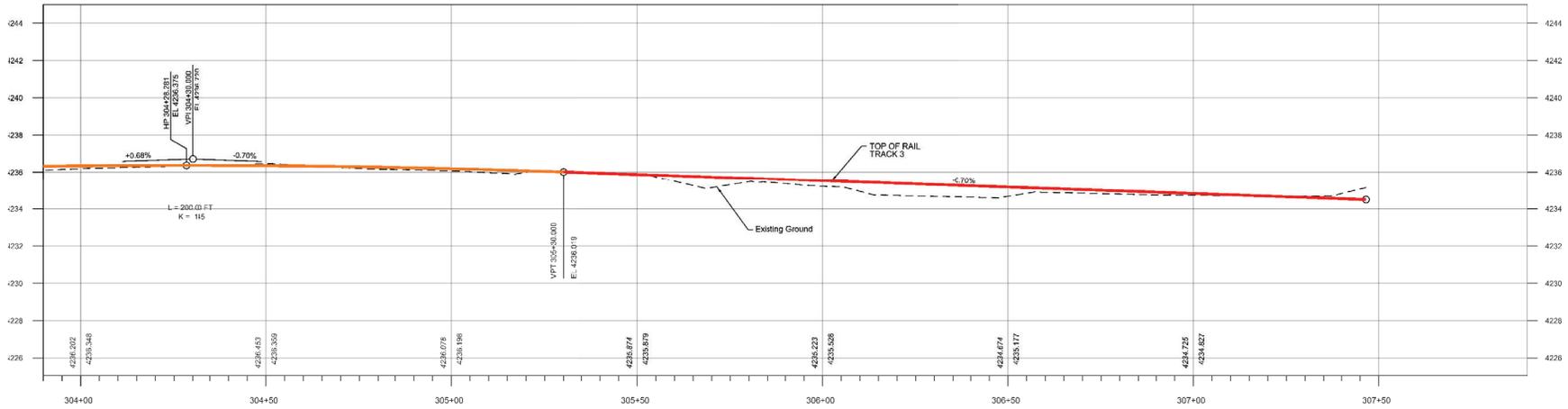
UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

TRACK PROFILE  
SHEET 1 OF 2

Scale:	AS SHOWN
CADD Filename:	TRACK PROFILE SHEET
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	TRP-01
Sheet No.:	7



TRACK 3 PROFILE



TRACK 3 PROFILE

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
△		
△		
△		
REV	DATE	Description

Submitted By: \_\_\_\_\_



Approved By: \_\_\_\_\_

Designed By:	BH
Drawn By:	BH
Checked By:	GT
Approved By:	JS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

TRACK PROFILE  
SHEET 2 OF 2

Scale:	AS SHOWN
CADD Filename:	TRACK PROFILE SHEET
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	TRP-02
Sheet No.:	8



KEY NOTES (THIS SHEET):

- ① VEHICLE GATE MECHANISM. SEE REFERENCE SHEETS.
- ② 4" SOLID WHITE STRIPE
- ③ 4" DOUBLE YELLOW STRIPE
- ④ 12" WHITE STOP BAR
- ⑤ LAYOUT REFERENCE POINT = 306+00.00

NOTES:

- 1. SEE TRANSITION SLAB DETAIL, SHEET 2



△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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△		
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REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_



Designed By:	BH
Drawn By:	BH
Checked By:	GT
Approved By:	JS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT  
  
GRADE CROSSING DETAIL  
SHEET 1 OF 1

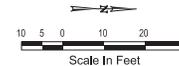
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Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	GRDT-01
Sheet No.:	9



POINT DATA			
POINT #	STATION	OFFSET	ACTION
1	100+00.00	6.00 RT	MATCH EXISTING SLAB
2	100+95.34	6.00 RT	MATCH EXISTING SLAB
3	102+07.56	17.39 RT	MATCH EXISTING SLAB, PROTECT IN PLACE INLET STRUCTURE
4	102+13.89	11.06 RT	MATCH EXISTING SLAB, PROTECT IN PLACE INLET STRUCTURE
5	102+20.48	16.56 RT	MATCH EXISTING SLAB, PROTECT IN PLACE INLET STRUCTURE
6	102+92.74	7.20 RT	MATCH EXISTING SLAB
7	101+87.85	9.00 LT	MATCH EXISTING SLAB
8	102+13.15	9.80 LT	MATCH EXISTING SLAB
9	102+90.79	6.00 LT	MATCH EXISTING SLAB

NOTES:

1. SEE TRACK TYPICAL SECTIONS FOR SLAB PAVEMENT SECTION.
2. MATCH EMBEDDED TRACK SLAB ELEVATION AT INSIDE EDGE.
3. TRACK SLAB ELEVATION SET BY TRACK 1 PROFILE. SEE TRACK PROFILE SHEETS.
4. SAWCUT EXISTING SLAB AS SHOWN.



△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

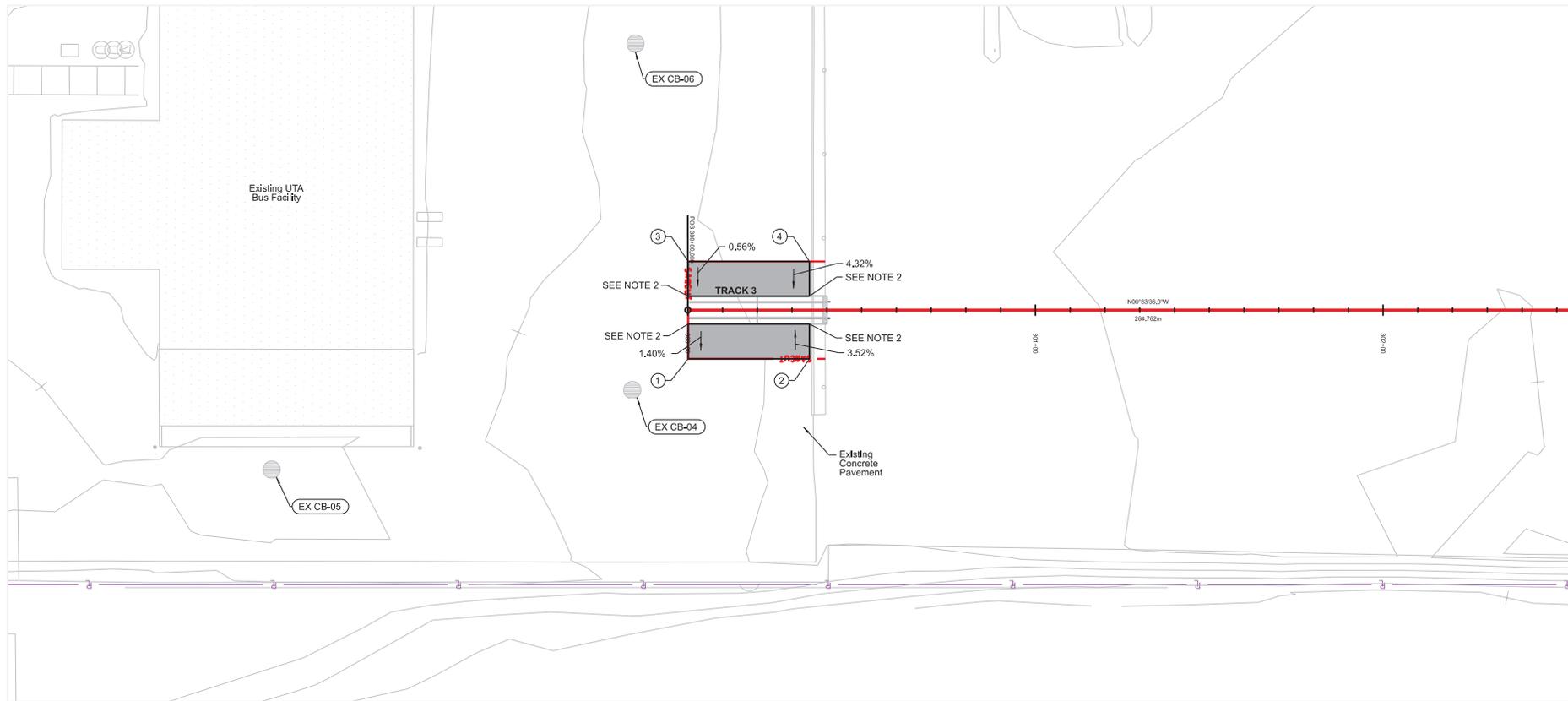


Designed By:	BH
Drawn By:	BH
Checked By:	GT
Approved By:	JS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

TRACK SLAB DETAIL  
SHEET 1 OF 2

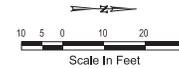
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Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	TRDT-01
Sheet No.:	10



POINT DATA				
POINT #	STATION	OFFSET		ACTION
1	300+00.00	14.00 RT		MATCH EXISTING SLAB
2	300+35.00	14.00 RT		MATCH EXISTING SLAB
3	300+00.00	14.00 LT		MATCH EXISTING SLAB
4	300+35.00	14.00 LT		MATCH EXISTING SLAB

NOTES:

1. SEE TRACK TYPICAL SECTIONS FOR SLAB PAVEMENT SECTION.
2. MATCH EMBEDDED TRACK SLAB ELEVATION AT INSIDE EDGE.
3. TRACK SLAB ELEVATION SET BY TRACK 3 PROFILE. SEE TRACK PROFILE SHEETS.
4. SAWCUT EXISTING SLAB AS SHOWN.



△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

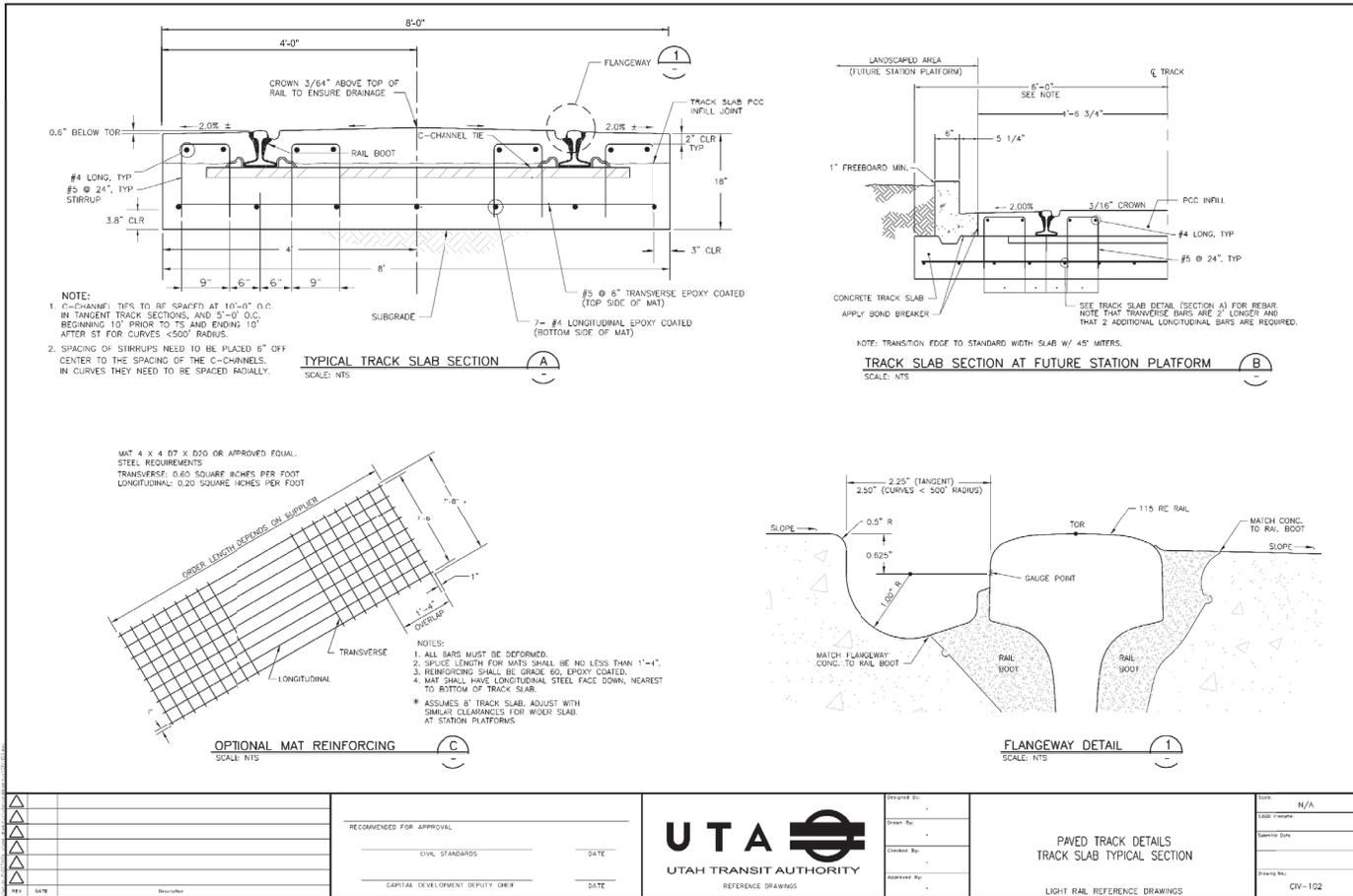


Designed By:	BH
Drawn By:	BH
Checked By:	GT
Approved By:	JS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

TRACK SLAB DETAIL  
SHEET 2 OF 2

Scale:	AS SHOWN
CADD Filename:	TRACK SLAB DETAIL SHEET
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	TRDT-02
Sheet No.:	11



FOR REFERENCE ONLY

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_



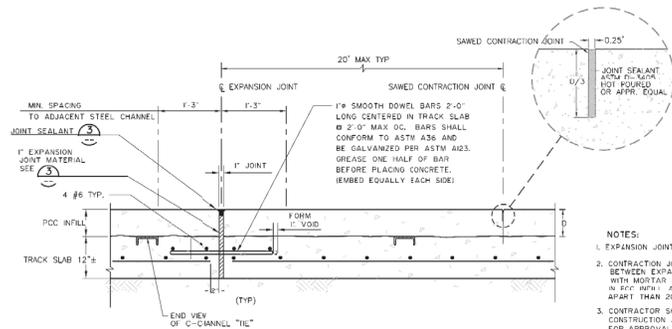
PAVED TRACK DETAILS  
TRACK SLAB TYPICAL SECTION

LIGHT RAIL REFERENCE DRAWINGS

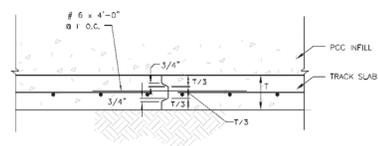
UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

REFERENCE DRAWING  
SHEET 1

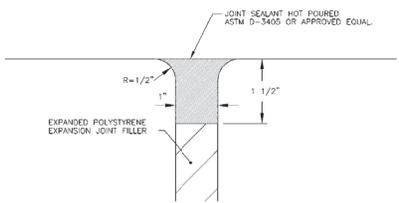
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CADD Filename:	REFERENCE SHEET 01
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	TRRF-01
Sheet No.:	12



EXPANSION JOINT DETAIL (1)  
SCALE: NTS



CONSTRUCTION JOINT DETAIL (2)  
SCALE: NTS



JOINT SEALANT DETAIL (3)  
SCALE: NTS

- NOTES:
1. EXPANSION JOINT SPACING 60\"/>
  - 2. CONTRACTION JOINTS TO BE SPACED EVENLY BETWEEN EXPANSION JOINTS AND TO ALIGN WITH MORTAR JOINTS OF TEXTURED PATTERNS IN PCC INFILL AND 9001 REF AND F102H77 APART 1MAN 20 FEET.
  - 3. CONTRACTOR SHALL SUBMIT AN EXPANSION AND CONTRACTION JOINT LAYOUT TO THE ENGINEER FOR APPROVAL PRIOR TO CONSTRUCTION.
  - 4. FOR TRACK SLAB DETAILS, SEE (A)

	RECOMMENDED FOR APPROVAL	<p>UTAH TRANSIT AUTHORITY REFERENCE DRAWINGS</p>	DESIGNED BY: - DRAWN BY: - CHECKED BY: - APPROVED BY: -	PAVED TRACK DETAILS TRACK SLAB STRUCTURAL DETAILS LIGHT RAIL REFERENCE DRAWINGS	DATE	SCALE	DATE	SCALE	DATE	SCALE
	CIVIL STANDARDS				DATE	SCALE	DATE	SCALE		
	CAPITAL DEVELOPMENT DEPUTY CHIEF				DATE	SCALE	DATE	SCALE		
					DATE	SCALE	DATE	SCALE		

FOR REFERENCE ONLY

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
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REV	DATE	Description

Submitted By: _____	Date: _____
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Approved By: _____	Date: _____
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Designed By: BH
Drawn By: BH
Checked By: GT
Approved By: JS

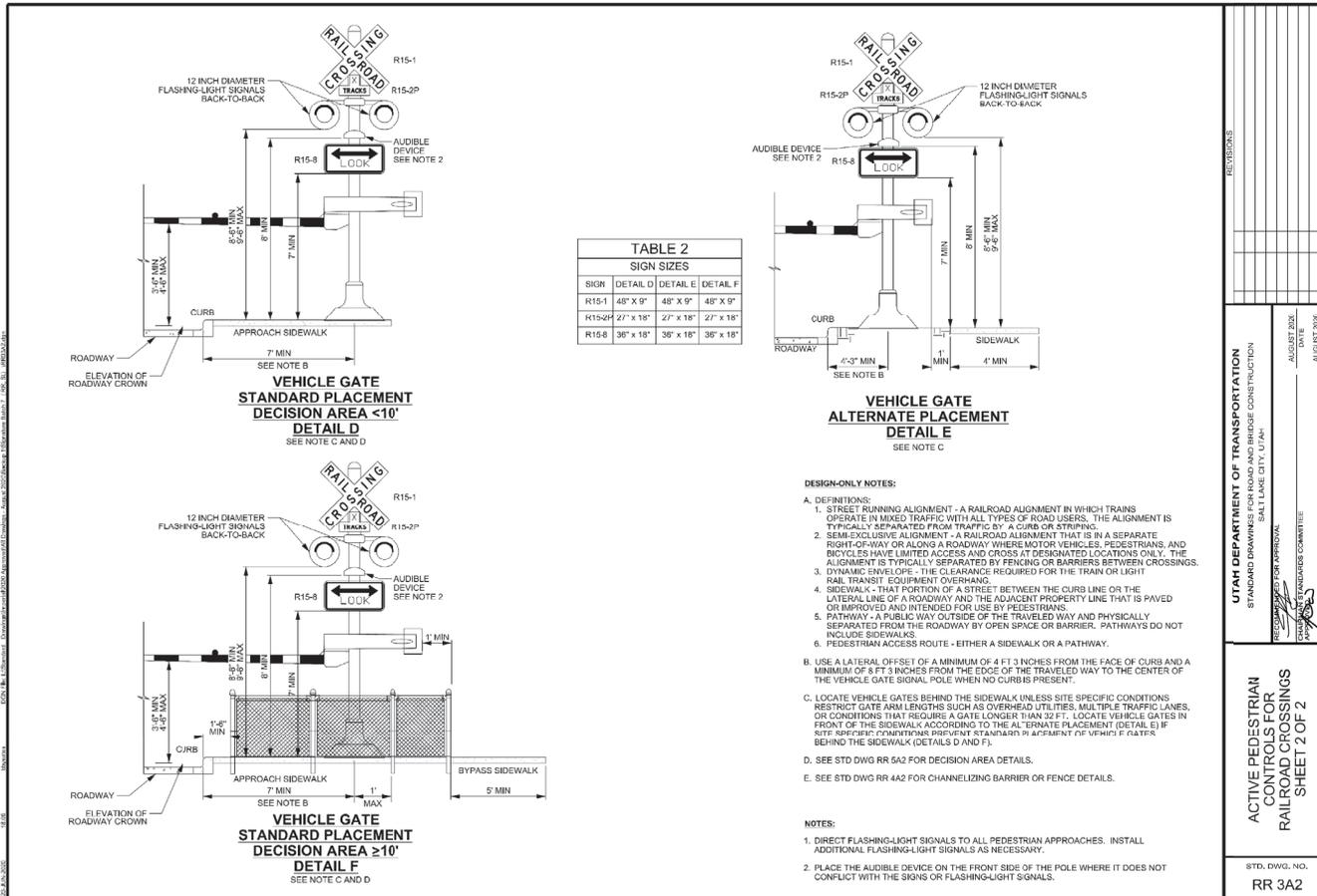
UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

REFERENCE DRAWING  
SHEET 2

Scale: AS SHOWN
CADD Filename: REFERENCE SHEET 02
Submitter Date: 05/08/23
UTA Contract No.:
Drawing No.: TRRF-02
Sheet No.: 13







FOR REFERENCE ONLY

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
△		
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△		
REV	DATE	Description

Submitted By: \_\_\_\_\_



Designed By: BH  
 Drawn By: BH  
 Checked By: GT  
 Approved By: JS

UTA FACILITIES  
 M.O.W. TRAINING YARD  
 823 W DAVIS RD, SOUTH SALT LAKE, UT

REFERENCE DRAWING  
 SHEET 5

Scale: AS SHOWN  
 CADD Filename: REFERENCE SHEET 05  
 Submit Date: 05/08/23  
 UTA Contract No.:  
 Drawing No.: TRRF-05  
 Sheet No.: 16

REVISIONS

NO.	DATE	BY	REVISION

UTAH DEPARTMENT OF TRANSPORTATION  
 STANDARD DRAWING FOR CONSTRUCTION  
 SALT LAKE CITY, UTAH

DESIGNED BY: \_\_\_\_\_  
 DRAWN BY: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_  
 APPROVED BY: \_\_\_\_\_

DESIGNED DATE: \_\_\_\_\_  
 DRAWN DATE: \_\_\_\_\_  
 CHECKED DATE: \_\_\_\_\_  
 APPROVED DATE: \_\_\_\_\_

ACTIVE PEDESTRIAN CONTROLS FOR RAILROAD CROSSINGS SHEET 2 OF 2  
 STANDARD DRAWING TITLE

STD. DWG. NO. RR 3A2

**GENERAL NOTES:**

1. SEE SHEET DRDT-01 FOR DRAINAGE STRUCTURE LAYOUT DETAILS.
2. ALL UTILITY LOCATIONS ARE APPROXIMATE AND SUBJECT TO FIELD VERIFICATION.
3. PIPE LENGTH SHOWN ON DRAINAGE PROFILE SHEETS IS HORIZONTAL LENGTH MEASURED FROM INSIDE FACE OF UPSTREAM STRUCTURE TO INSIDE FACE OF DOWNSTREAM STRUCTURE.
4. WHERE A DISCREPANCY EXISTS ON PLAN BETWEEN PIPE SLOPE AND INVERT ELEVATIONS, INVERT ELEVATIONS SHALL GOVERN
5. ALL EXISTING PIPE ELEVATIONS AND LOCATIONS ARE APPROXIMATE AND SUBJECT TO FIELD VERIFICATION.
6. APPLY THE APPLICABLE BEST MANAGEMENT PRACTICES (BMP'S) FOR TEMPORARY EROSION CONTROL IN ACCORDANCE WITH THE EROSION CONTROL PLAN SHEETS AND THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP). CONTRACTOR TO COORDINATE WITH EROSION CONTROL SUPERVISOR AS NEEDED.
7. MAINTAIN AND KEEP EXISTING STORM DRAIN SYSTEMS OPERATIONAL AS NEEDED TO PROVIDE DRAINAGE DURING CONSTRUCTION
8. PROTECT IN PLACE ALL EXISTING STORM DRAIN/IRRIGATION FACILITIES UNLESS OTHERWISE NOTED.
9. NOTIFY AND COORDINATE WITH ALL THIRD PARTY AGENCIES/ENTITIES PRIOR TO ANY WORK PERFORMED ON ANY THIRD PARTY/ENTITY FACILITIES.
10. FOR ADDITIONAL DRAINAGE NOTES, SEE DRAINAGE PLAN, PROFILE, AND DETAIL SHEETS.

GENERAL DRAINAGE PROFILE NOTES:

1. CONTRACTOR SHALL TAKE ALL PRECAUTIONARY MEASURE TO PROTECT EXISTING UTILITIES AND IMPROVEMENTS THAT ARE TO REMAIN IN PLACE FROM DAMAGE.
2. THE LOCATIONS OF EXISTING UTILITIES, DITCHES, CONDUITS, AND UNDERGROUND STRUCTURES SHOWN ON THE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF, AND PRESERVING, ALL EXISTING UTILITIES IMPACTED BY CONSTRUCTION OF NEEDED PROJECT IMPROVEMENTS.
3. PROTECTING AND RELOCATING EXISTING UTILITIES SHALL BE PERFORMED IN ACCORDANCE WITH UTA, CITY, AND APWA SPECIFICATIONS.
4. CONSTRUCTION OF STORM DRAIN FACILITIES IN DEVELOPED AREAS MAY IMPACT SERVICE LATERALS OR SERVICE LINES. CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFYING THE LOCATIONS OF ALL SERVICE LINES AND LATERALS. ALL SERVICE LATERALS AND CONNECTIONS THAT NEED TO BE REPLACED DUE TO CONSTRUCTION CONFLICTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE STANDARDS OF THE THIRD PARTY OWNER.
5. CONTRACTOR SHALL FIELD VERIFY LOCATION AND DEPTHS OF BURIED UTILITIES IN THE FIELD BY POT-HOLING HEAD OF CONSTRUCTION IN AN EFFORT TO MINIMIZE CONFLICTS WITH DESIGNED PIPELINE GRADE AND ALIGNMENT. IF NEEDED, CONTRACTOR SHALL DEVELOP AND REQUEST A FIELD DESIGN CHANGE TO ADJUST LINE AND/OR GRADE OF NEW STORM DRAIN FACILITIES TO AVOID UTILITY CONFLICTS WHERE POSSIBLE.

DRAINAGE PLAN & PROFILE NOTES:

- 8 CONNECT PROPOSED PIPE TO EXISTING STRUCTURE. CORE AND GROUT A WATERTIGHT CONNECTION. SEE DETAIL A ON SHEET DRDT-01
- 9 SEE SHEET DRDT-01 FOR RIPRAP DETAILS.
- 10 SEE SHEETS GR-01 AND GR-02 FOR DITCH DETAILS.
- 11 SEE SHEET DRDT-02 FOR POND OUTLET STRUCTURE DETAILS.
- 12 SEE SHEETS GR-01 AND GR-02 FOR POND GRADING DETAILS.

XX-YY GENERAL DRAINAGE CALLOUT  
 XX = TYPE OF STRUCTURE  
 YY = STRUCTURE NUMBER

△	10/13/22	60% DESIGN
△	1/13/23	100% DESIGN
△	5/08/23	RFC
△		
△		
△		
REV	DATE	Description

Submitted By: \_\_\_\_\_



Designed By:	TTJ
Drawn By:	TTJ
Checked By:	RTS
Approved By:	RTS

UTA FACILITIES  
 M.O.W. TRAINING YARD  
 823 W DAVIS RD, SOUTH SALT LAKE, UT

DRAINAGE GENERAL NOTES  
 SHEET 1 OF 2

Scale:	NTS
CADD Filename:	MOV_DR_GN_NOTES
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	DRDT-00A
Sheet No.:	17

**PLAN LEGEND:**

- EXISTING CONTOUR
- sd - EXISTING STORM DRAIN
- PROPOSED CATCH BASIN /OUTLET STRUCTURE/SQUARE MANHOLE
- EXISTING MANHOLE/HYDRODYNAMIC SEPARATOR
- SD - PROPOSED STORM DRAIN
- FLOW DIRECTION ARROW
- PROPOSED CONTOUR
-  RIPRAP
-  PROPOSED END SECTION
-  PROPOSED SAFETY END SECTION
-  PROPOSED HEADWALL
- x— EXISTING FENCE
- PROPOSED DITCH

**PROFILE LEGEND:**

- FINISHED GRADE
- EXISTING GRADE
- HGL 10-YR (UNLESS NOTED OTHERWISE)
- ===== PROPOSED PIPE
- ===== EXISTING PIPE

**ABBREVIATIONS:**

- BCTV - BURIED CABLE TELEVISION
  - BE - BURIED ELECTRIC
  - BTEL - BURIED TELEPHONE
  - CB - CATCH BASIN
  - CH - CHANNEL
  - ES - END SECTION
  - EX - EXISTING
  - FO - FIBER OPTIC
  - G - GAS
  - OS - OUTLET STRUCTURE (DETENTION)
  - P - PIPE
  - PIP - PROTECT IN PLACE
  - RCP - REINFORCED CONCRETE PIPE
  - SLRP - SMOOTH, LEAK-RESISTANT PIPE
  - SS - SANITARY SEWER
  - WTR - WATER
- NOTE: EXISTING UTILITIES ARE NOTES IN LOWER CASE LETTERS AND PROPOSED UTILITIES ARE NOTES IN UPPER CASE LETTERS.

	10/13/22	60% DESIGN
	12/23/22	100% DESIGN
	5/08/23	RFC
		
		
		
REV	DATE	Description

Submitted By: \_\_\_\_\_



Designed By:	TTJ
Drawn By:	TTJ
Checked By:	RTS
Approved By:	RTS

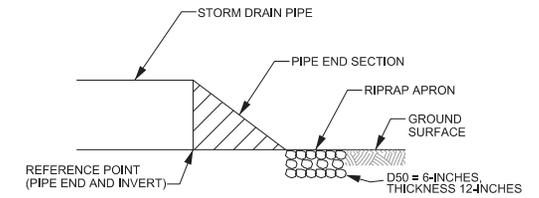
UTA FACILITIES  
 M.O.W. TRAINING YARD  
 823 W DAVIS RD, SOUTH SALT LAKE, UT

DRAINAGE GENERAL NOTES  
 SHEET 2 OF 2

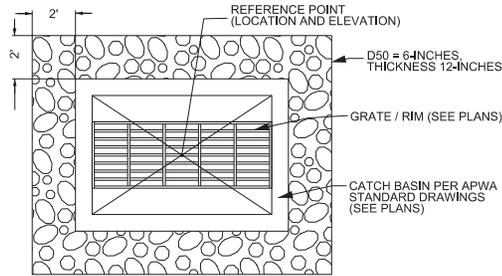
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Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	DRDT-00B
Sheet No.:	18

STRUCTURE ID	PIPE DIAMETER		LOOSE RIPRAP D50	LENGTH OF APRON			WIDTH OF APRON			DEPTH OF APRON			LOOSE RIPRAP	SEPARATION GEOTEXTILE	REMARKS
	(D)	(IN)		(L)	(Y)	(N)	(CY)	(SY)							
ES-01	15	12	12	5	24	4.4	16.2	LAYOUT C							
ES-04	15	12	28	16	48	66.4	93.9	LAYOUT C							
ES-10	15	6	10	5	24	3.7	14.0	LAYOUT C							
ES-11	15	12	30	18	48	80.0	108.1	LAYOUT C							
ES-02	15	6	10	5	24	3.7	14.0	LAYOUT C							
ES-06	15	6	8	5	24	3.0	11.8	LAYOUT C							
ES-03	15	6	8	5	24	3.0	11.8	LAYOUT C							
ES-07	15	6	8	5	24	3.0	11.8	LAYOUT C							

**RIPRAP DETAILS**

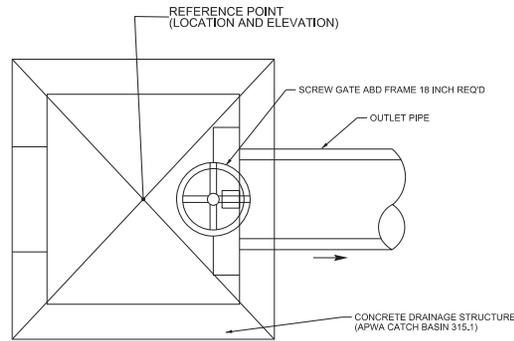


**LAYOUT B - PIPE END SECTION OR HEADWALL PLACEMENT**

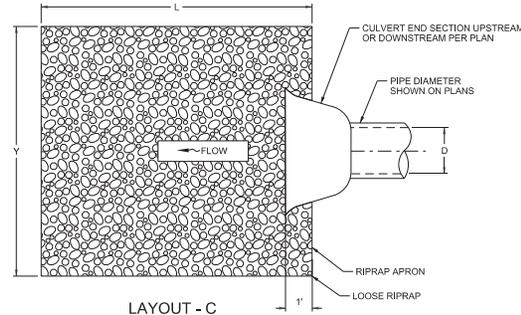


NOTE: CATCH BASIN / MANHOLE / STATION OFFSET AND GRATE / RIM ELEVATION ARE CALLED OUT TO THE CENTER OF BOX

**PLAN VIEW - CATCH BASIN / MANHOLE**

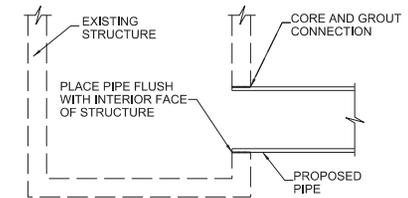


**PLAN VIEW - POND OUTLET STRUCTURE**

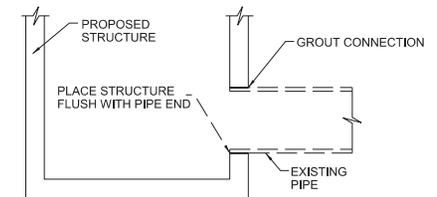


**LAYOUT - C**

**LAYOUT D - DRAINAGE STRUCTURE PLACEMENT FOR FIELD STRUCTURE**



**CORE AND GROUT CONNECTION TO EXISTING STRUCTURE**



**GROUT CONNECTION TO EXISTING PIPE**

**NOTES:**

1. MAXIMUM CORE HOLE DIAMETERS IS OUTSIDE PIPE DIAMETER + 4".
2. IF EXISTING HOLE DIAMETER >4", USE A CONCRETE COLLAR ON THE EXTERIOR OF THE BOX WITH NON-SHRINK GROUT ON THE INTERIOR OF THE BOX. NON-SHRINK GROUT TO CONFORM TO APWA 2017 SPECIFICATIONS.
3. ALL CORE AND GROUT WORK IS INCIDENTAL TO PIPE.

**DETAIL A CORE AND GROUT CONNECTIONS**

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	5/08/23	RFC
△		
△		
△		
REV	DATE	Description

Submitted By: \_\_\_\_\_

Approved By: \_\_\_\_\_

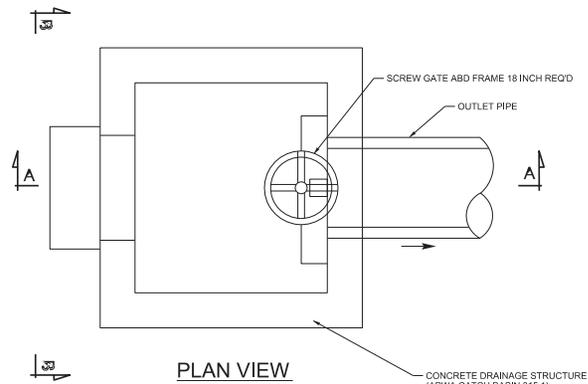


Designed By:	TTJ
Drawn By:	TTJ
Checked By:	RTS
Approved By:	RTS

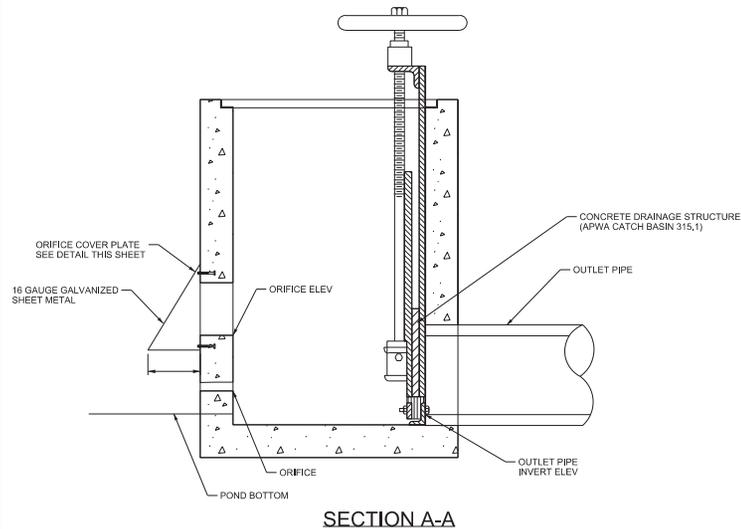
UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

DRAINAGE DETAILS  
SHEET 1 OF 2

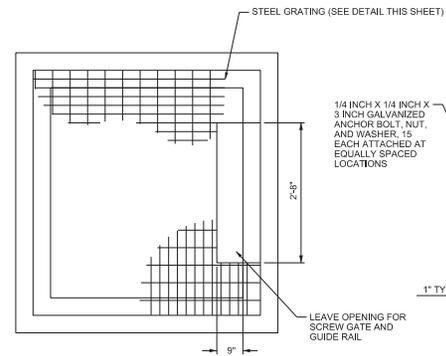
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Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	DRDT-01
Sheet No.:	19



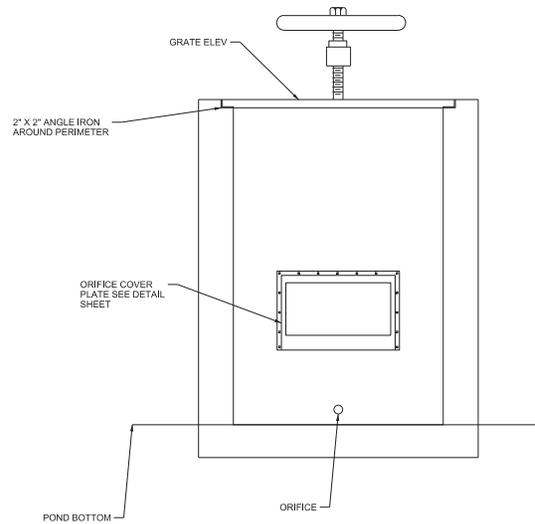
PLAN VIEW



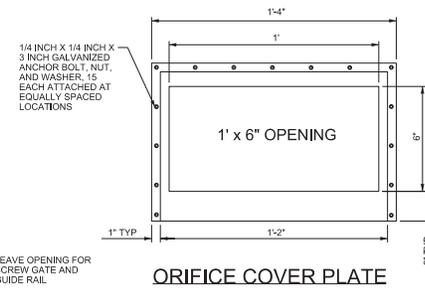
SECTION A-A



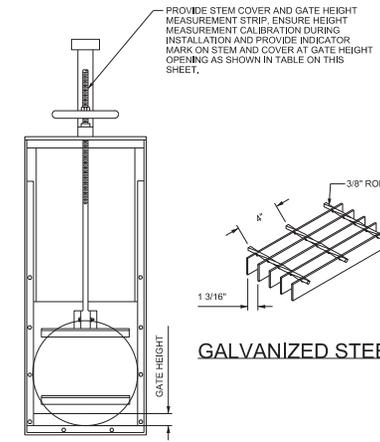
GRATING DETAIL



SECTION B-B



ORIFICE COVER PLATE



SCREW GATE ORIFICE

GALVANIZED STEEL GRATING

NOTES:

1. GRATING IS INCIDENTAL TO POND OUTLET STRUCTURE.
2. SEE APWA CATCH BASIN 315.1 PER APWA 2017 STD DWGS AND INSTALL GRATING PER DETAIL THIS SHEET.

DETENTION POND OUTLET STRUCTURE DETAILS

POND	OUTLET STRUCTURE ID	BOTTOM POND ELEV	GRATE ELEV	ORIFICE ELEV	ORIFICE SIZE (IN.)	OUTLET PIPE INVERT	GATE HEIGHT (IN.)
WEST	OS-13	4229.69	4233.00	4230.50	2.50"	4229.74	3"
EAST	OS-12	4229.10	4232.50	4229.50	2.40"	4229.36	3"

REV	DATE	Description
△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	5/08/23	RFC
△		
△		
△		

Submitted By: \_\_\_\_\_ Approved By: \_\_\_\_\_

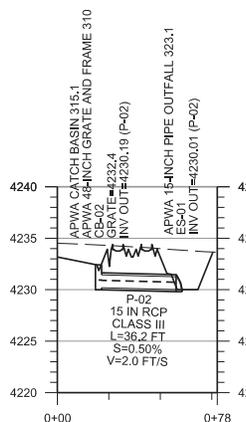


Designed By:	TTJ
Drawn By:	TTJ
Checked By:	RTS
Approved By:	RTS

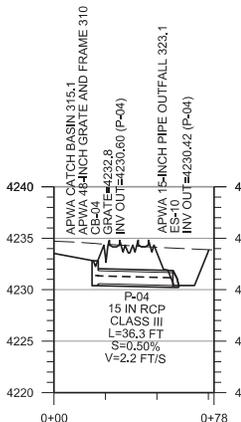
UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

DRAINAGE DETAILS  
SHEET 2 OF 2

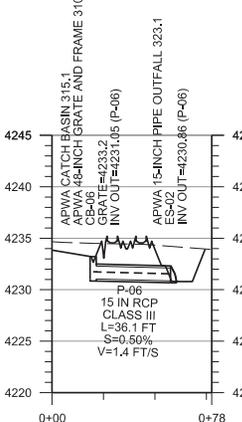
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Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	DRDT-02
Sheet No.:	20



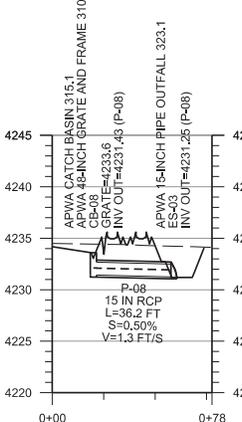
CB-02 TO ES-01



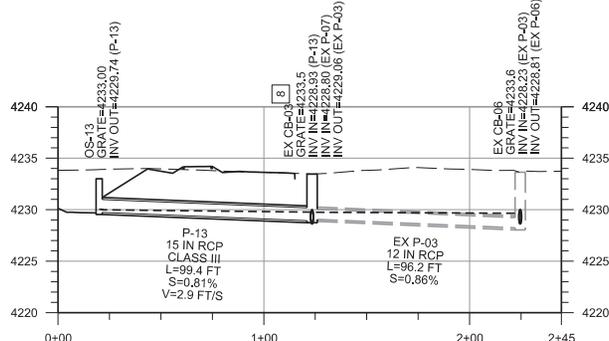
CB-04 TO ES-10



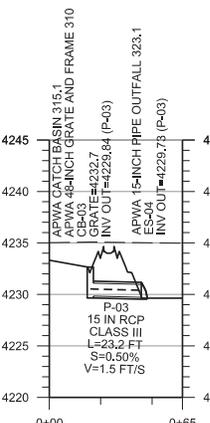
CB-06 TO ES-02



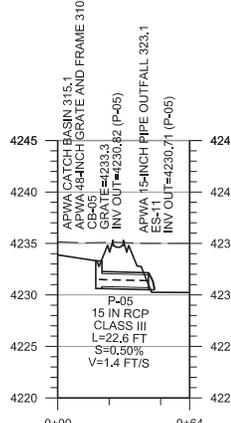
CB-08 TO ES-03



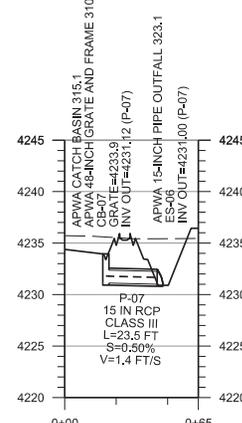
OS-13 TO EX CB-06



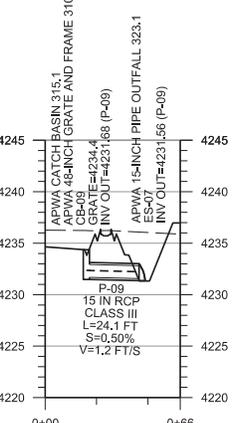
CB-03 TO ES-04



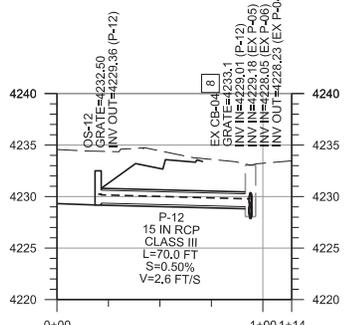
CB-05 TO ES-11



CB-07 TO ES-06



CB-09 TO ES-07



OS-12 TO EX CB-04

△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	5/08/23	RFC
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REV	DATE	Description

Submitted By: \_\_\_\_\_



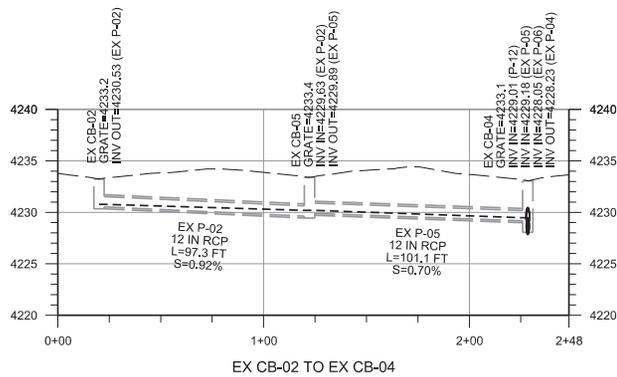
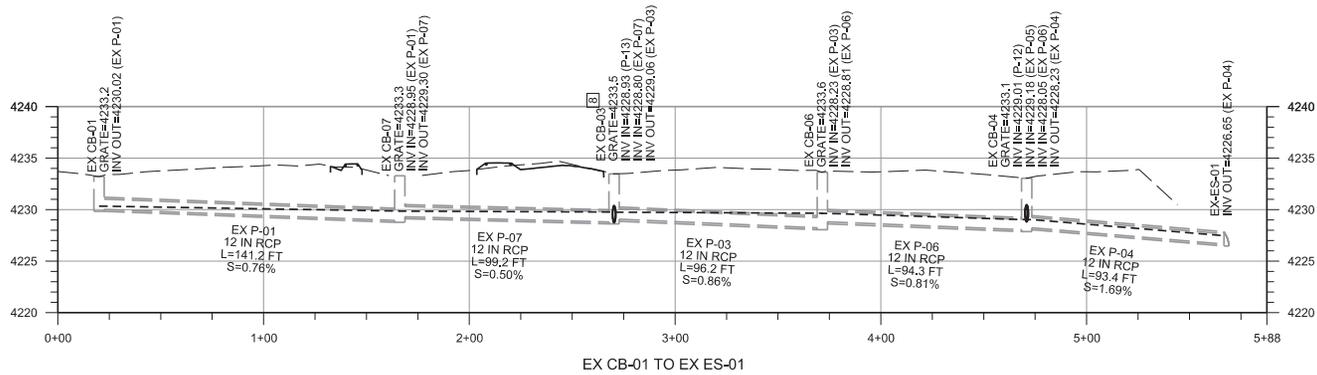
Approved By: \_\_\_\_\_

Designed By:	TTJ
Drawn By:	TTJ
Checked By:	RTS
Approved By:	RTS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

DRAINAGE PROFILES  
SHEET 1 OF 2

Scale:	1:60
CADD Filename:	MOV_DR_PROFILES
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	DP-01
Sheet No.:	21



△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	5/08/23	RFC
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REV	DATE	Description

Submitted By: \_\_\_\_\_



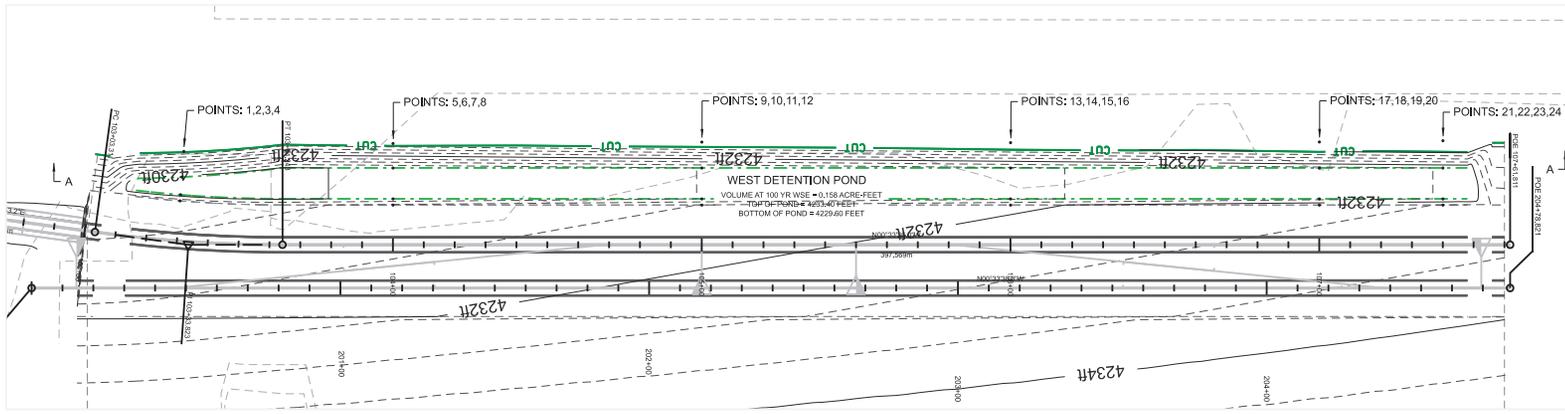
Approved By: \_\_\_\_\_

Designed By:	TTJ
Drawn By:	TTJ
Checked By:	RTS
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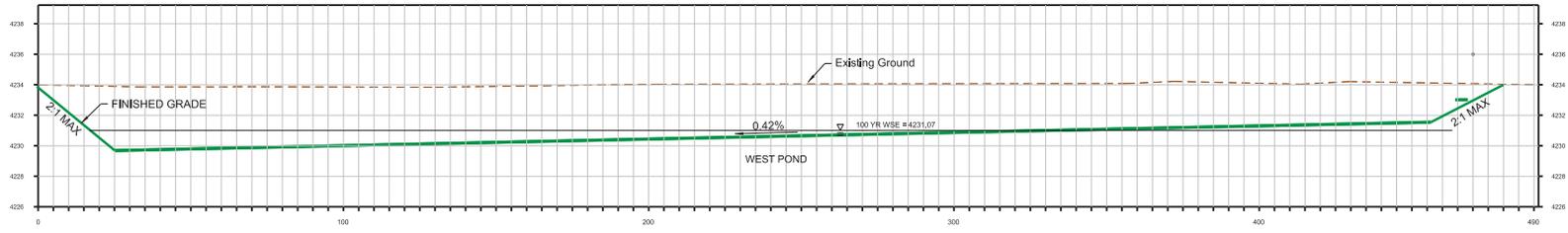
UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

DRAINAGE PROFILES  
SHEET 2 OF 2

Scale:	1:60
CADD Filename:	MOW_DR_PROFILES
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	DP-02
Sheet No.:	22



POINT #	ALIGN	STA	OFF	ELEV
1	TRACK 1	103+30.00	12.95 LT	4230.64
2	TRACK 1	103+30.00	14.95 LT	4229.64
3	TRACK 1	103+30.00	20.68 LT	4229.64
4	TRACK 1	103+30.00	29.00 LT	4233.80
5	TRACK 1	104+00.00	12.86 LT	4230.68
6	TRACK 1	104+00.00	14.86 LT	4229.68
7	TRACK 1	104+00.00	24.86 LT	4229.68
8	TRACK 1	104+00.00	32.78 LT	4233.65
9	TRACK 1	105+00.00	12.86 LT	4230.70
10	TRACK 1	105+00.00	14.86 LT	4229.70
11	TRACK 1	105+00.00	24.86 LT	4229.70
12	TRACK 1	105+00.00	33.24 LT	4233.89
13	TRACK 1	106+00.00	12.86 LT	4230.71
14	TRACK 1	106+00.00	14.86 LT	4229.71
15	TRACK 1	106+00.00	24.86 LT	4229.71
16	TRACK 1	106+00.00	33.06 LT	4233.82
17	TRACK 1	107+00.00	12.86 LT	4230.73
18	TRACK 1	107+00.00	14.86 LT	4229.73
19	TRACK 1	107+00.00	24.86 LT	4229.73
20	TRACK 1	107+00.00	33.28 LT	4233.94
21	TRACK 1	107+40.00	12.86 LT	4230.74
22	TRACK 1	107+40.00	14.86 LT	4229.74
23	TRACK 1	107+40.00	24.86 LT	4229.74
24	TRACK 1	107+40.00	33.68 LT	4234.15



△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
△		
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△		
REV	DATE	Description

Submitted By: \_\_\_\_\_



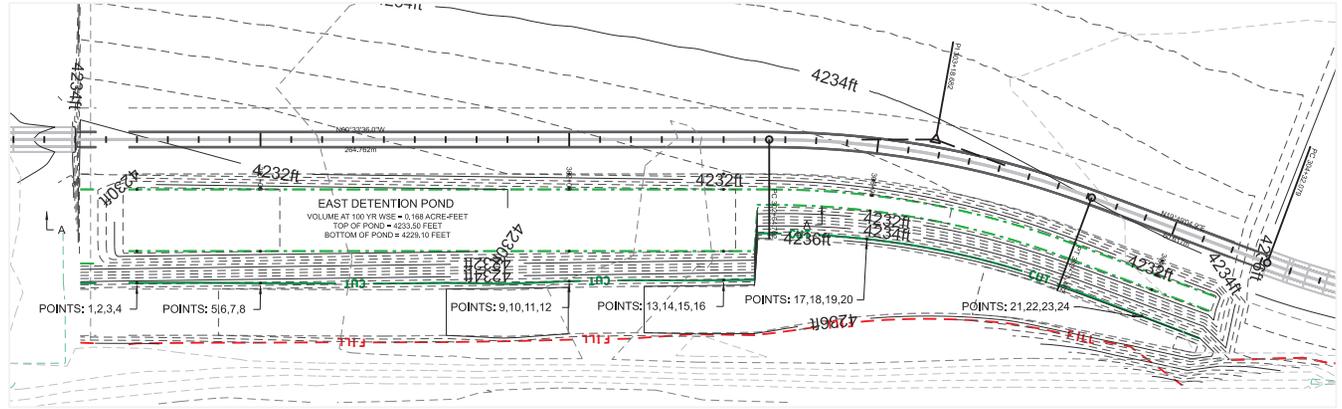
Designed By: BH, TJ  
 Drawn By: BH, TJ  
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 Approved By: JS

UTA FACILITIES  
 M.O.W. TRAINING YARD  
 823 W DAVIS RD, SOUTH SALT LAKE, UT

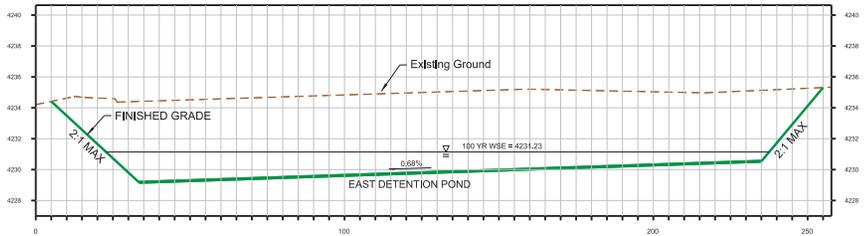
DRAINAGE POND PLAN AND PROFILE  
 SHEET 1 OF 2

Scale:	AS SHOWN
CADD Filename:	POND PLAN & PROFILES SHEET 01
Submit Date:	05/08/23
UTA Contract No.:	
Drawing No.:	GR-01
Sheet No.:	23

POINT #	ALIGN	STA	OFF	ELEV
1	TRACK 3	300+60.00	11.12 RT	4231.69
2	TRACK 3	300+60.00	16.12 RT	4229.19
3	TRACK 3	300+60.00	36.12 RT	4229.19
4	TRACK 3	300+60.00	46.35 RT	4234.30
5	TRACK 3	301+00.00	11.12 RT	4231.96
6	TRACK 3	301+00.00	16.12 RT	4229.46
7	TRACK 3	301+00.00	36.12 RT	4229.46
8	TRACK 3	301+00.00	46.46 RT	4234.63
9	TRACK 3	302+00.00	11.12 RT	4232.64
10	TRACK 3	302+00.00	16.12 RT	4230.14
11	TRACK 3	302+00.00	36.12 RT	4230.14
12	TRACK 3	302+00.00	45.83 RT	4234.99
13	TRACK 3	302+50.00	11.12 RT	4232.97
14	TRACK 3	302+50.00	16.12 RT	4230.47
15	TRACK 3	302+50.00	36.12 RT	4230.47
16	TRACK 3	302+50.00	45.49 RT	4235.17
17	TRACK 3	303+00.00	11.12 RT	4233.31
18	TRACK 3	303+00.00	16.12 RT	4230.81
19	TRACK 3	303+00.00	21.12 RT	4230.81
20	TRACK 3	303+00.00	30.28 RT	4235.39
21	TRACK 3	304+00.00	11.12 RT	4233.82
22	TRACK 3	304+00.00	16.12 RT	4231.32
23	TRACK 3	304+00.00	21.12 RT	4231.32
24	TRACK 3	304+00.00	30.28 RT	4235.90

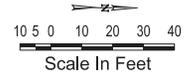


**EAST DETENTION POND PLAN**



**SECTION A-A**

H: 1"=8'  
V: 1"=40'



REV	DATE	Description
△	10/13/22	60% DESIGN
△	12/23/22	100% DESIGN
△	05/08/23	RFC
△		
△		

Submitted By: \_\_\_\_\_



Designed By:	BH, TJ
Drawn By:	BH, TJ
Checked By:	GT, RS
Approved By:	JS

UTA FACILITIES  
M.O.W. TRAINING YARD  
823 W DAVIS RD, SOUTH SALT LAKE, UT

**DRAINAGE POND PLAN AND PROFILE**  
SHEET 2 OF 2

Scale:	AS SHOWN
CADD Filename:	POND PLAN & PROFILES SHEET 02
Submitter Date:	05/08/23
UTA Contract No.:	
Drawing No.:	GR-02
Sheet No.:	24

JUNCTION BOX ASSIGNMENT		
BOX NUMBER	USE	BOX TYPE
PB01	SIGNAL/COMMS	5
PB02	SIGNAL/COMMS	5
PB03	SIGNAL/COMMS	5
PB04	SIGNAL/COMMS	3
PB05	SIGNAL/COMMS	2
PB06	SIGNAL/COMMS	7
PB07	SIGNAL/COMMS	7
PB08	SIGNAL/COMMS	7
PB09	SIGNAL/COMMS	7
PB10	SIGNAL/COMMS	7
PB10.5	SIGNAL/COMMS	7
PB11	SIGNAL/COMMS	7
PB12	SIGNAL/COMMS	7
PB15	SIGNAL/COMMS	5
PB16	SIGNAL/COMMS	7
PB18	SIGNAL/COMMS	5
PB19	SIGNAL/COMMS	7
PB20.5	SIGNAL/COMMS	2
PB21	SIGNAL/COMMS	5
PB22	SIGNAL/COMMS	1
PB23	SIGNAL/COMMS	1
PB24	SIGNAL/COMMS	7
PB25	SIGNAL/COMMS	1
PB26	SIGNAL/COMMS	5
PB27	SIGNAL/COMMS	5
PB28	SIGNAL/COMMS	1
PB29	SIGNAL/COMMS	7
PB30	SIGNAL/COMMS	1
PB31	SIGNAL/COMMS	1
PB32	SIGNAL/COMMS	7
PB33	SIGNAL/COMMS	5
PB34	SIGNAL/COMMS	5
PB39	SIGNAL/COMMS	1
PB40	SIGNAL/COMMS	1
PB41	SIGNAL/COMMS	1
PB42	SIGNAL/COMMS	1
PB43	SIGNAL/COMMS	1
PB44	SIGNAL/COMMS	1
PB45	SIGNAL/COMMS	1
PB46	SIGNAL/COMMS	1

JUNCTION BOX ASSIGNMENT		
BOX NUMBER	USE	BOX TYPE
PB13	OCS/LIGHTING	3
PB14	OCS/LIGHTING	2
PB15	OCS/LIGHTING	3
PB03	OCS/LIGHTING	3
PB17	OCS/LIGHTING	2
PB20	OCS/LIGHTING	2
PB33	OCS/LIGHTING	2
PB26	OCS/LIGHTING	2
PB27	OCS/LIGHTING	2
PB39	OCS/LIGHTING	2
PB40	OCS/LIGHTING	2
PB41	OCS/LIGHTING	2
PB43	OCS/LIGHTING	2

LEGEND  
 S=SIGNAL  
 C=COMMS  
 P=POWER  
 O=OCS  
 L=LIGHTING  
 DB=DUCT BANK  
 JB=JUNCT. BOX

JUNCTION BOX ASSIGNMENT		
BOX NUMBER	USE	BOX TYPE
PB35	POWER	2
PB36	POWER	2
PB37	POWER	2
PB38	POWER	2

TYPE	NOMINAL DIMENTIONS (IN INCHES)			
	A	B	C (TOP)	C (BOTTOM)
1	14	23	15	N/A
2	30	48	24	N/A
3	30	48	24	24
4	14 (OD)		10	N/A
5	36	64	36	N/A
6	36	64	36	36
7	14	20		SIGNAL CIRCUIT

TYPE	NOMINAL DIMENTIONS (IN INCHES)			
	A	B	C (TOP)	C (BOTTOM)
1	14	23	15	N/A
2	30	48	24	N/A
3	30	48	24	24
4	14 (OD)		10	N/A
5	36	64	36	N/A
6	36	64	36	36
7	14	20		SIGNAL CIRCUIT

INDIVIDUAL SIGNAL AND COMMS CONDUIT AND BANK TYPE (SC)

CONDUIT PATH				CONDUIT INFORMATION				
DUCT/BANK#	conduit #	From	To	Destination	Conduit Size	Conduit Contents	Circuit NAME	Bank Type
S1	S1-1	SIGNAL HOUSE	PULL BOX PB01	N SIGNALS/N TRACKS	1 4"	2 7C #14/4 #6 TP	N SIGNALS	Typical for 8 4"
S1	S1-2	SIGNAL HOUSE	PULL BOX PB01	8 5 /SOUTH TRACKS	1 4"	7C #14/4 #6 TP	8 5	
S1	S1-3	SIGNAL HOUSE	PULL BOX PB01	6 5/ OS TRACKS	1 4"	7C #14/2 #6 TP	6 5	
S1	S1-4	SIGNAL HOUSE	PULL BOX PB01	1 SWITCHES	1 4"	2 12C#14, 2 7C#6	TRACK AND SWITCHES	
S1	S1-5	SIGNAL HOUSE	PULL BOX PB01	3 SWITCHES	1 4"	2 12C#14, 2 7C#6	OS AND SWITCHES	
S1	S1-6	SIGNAL HOUSE	PULL BOX PB01	SW HEATER	1 4"	12C#14, 3C#2	SWH	
S1	S1-6	SIGNAL HOUSE	PULL BOX PB01	SPARE	1 4"	SPARE		
S1	S1-6	SIGNAL HOUSE	PULL BOX PB01	SPARE	1 4"	SPARE		
S2	S2-1	PULL BOX PB01	PULL BOX PB02	N SIGNALS/N TRACKS	1 4"	2 7C #14/4 #6 TP	N SIGNALS	Typical for 8 4"
S2	S2-2	PULL BOX PB01	PULL BOX PB02	8 5 /SOUTH TRACKS	1 4"	7C #14/4 #6 TP	8 5	
S2	S2-3	PULL BOX PB01	PULL BOX PB02	6 5/ OS TRACKS	1 4"	7C #14/2 #6 TP	6 5	
S2	S2-4	PULL BOX PB01	PULL BOX PB02	1 SWITCHES	1 4"	2 12C#14, 2 7C#6	TRACK AND SWITCHES	
S2	S2-5	PULL BOX PB01	PULL BOX PB02	3 SWITCHES	1 4"	2 12C#14, 2 7C#6	3 SWITCHES	
S2	S2-6	PULL BOX PB01	PULL BOX PB02	SW HEATER	1 4"	12C#14, 3C#2	SWH	
S2	S2-7	PULL BOX PB01	PULL BOX PB02	SPARE	1 4"	SPARE		
S2	S2-8	PULL BOX PB01	PULL BOX PB02	SPARE	1 4"	SPARE		
S3	S3-1	PULL BOX PB02	PULL BOX PB03	8 5 /SOUTH TRACKS	1 4"	7C #14/4 #6 TP	8 5	Typical for 6 4"
S3	S3-2	PULL BOX PB02	PULL BOX PB03	6 5/ OS TRACKS	1 4"	7C #14/2 #6 TP	6 5	
S3	S3-3	PULL BOX PB02	PULL BOX PB03	1A SWITCH	1 4"	12C#14, 7C#6	1A SW	
S3	S3-4	PULL BOX PB02	PULL BOX PB03	SW HEATER	1 4"	12C#14, 3C#2	SWH	
S3	S3-5	PULL BOX PB02	PULL BOX PB03	SPARE	1 4"	SPARE		
S3	S3-6	PULL BOX PB02	PULL BOX PB03	SPARE	1 4"	SPARE		
S4	S4-1	PULL BOX PB03	PULL BOX PB04	8 5 /SOUTH TRACKS	1 4"	7C #14/4 #6 TP	8 5	Typical for 6 4" (2 SPARE)
S4	S4-2	PULL BOX PB03	PULL BOX PB04	6 5/ OS TRACKS	1 4"	7C #14/2 #6 TP	6 5	
S4	S4-3	PULL BOX PB03	PULL BOX PB04	1A SWITCH	1 4"	12C#14, 7C#6	1 SW	
S4	S4-4	PULL BOX PB03	PULL BOX PB04	SW HEATER	1 4"	12C#14, 3C#2	SWH	
S5	S5-1	PULL BOX AD04	85 SIGNAL	85 SIGNAL	1 4"	7C #14	85	TYPICAL FOR 1 4"
S6	S6-1	PULL BOX PB04	PULL BOX PB05	6 5/ OS TRACKS	1 4"	7C #14/2 #6 TP	6 5	Typical for 5 4"
S6	S6-2	PULL BOX PB04	JB1A	1A SWITCH	1 4"	12C#14, 7C#6	1 SW	
S6	S6-3	PULL BOX PB04	SW HEATER	SW HEATER	1 4"	12C#14, 3C#2	SWH	
S6	S6-4	PULL BOX PB04	PULL BOX PB05	SPARE	1 4"	SPARE		
S6	S6-5	PULL BOX PB04	PULL BOX PB05	SPARE	1 4"	SPARE		
S7	S7-1	PULL BOX AD05	65 SIGNAL	6 5	1 4"	7C #14/2 #6 TP	6 5	TYPICAL FOR 2 4" AND 2 3"
S7	S7-2	PULL BOX AD05	2T PB10	2T	1 3"	#6 TP	2T TAPS	
S7	S7-3	PULL BOX AD05	1T PB11	1T	1 3"	#6 TP	1T TAPS	
S7	S7-4	PULL BOX AD05	SPARE	SPARE	1 4"	SPARE		
S9	S9-1	PULL BOX PB05	PULL BOX PB12	15T	1 3"	#6 TP	15T TAPS	TYPICAL FOR 3 3"
S9	S9-2	PULL BOX PB05	PULL BOX PB06	25T	1 3"	#6 TP	25T TAPS	
S9	S9-3	PULL BOX PB05	SPARE	SPARE	1 3"	SPARE		
S10	S10-1	PULL BOX PB06	PULL BOX PB07/08/09	25T	1 3"	#6 TP	25T TAPS	TYPICAL FOR 2 3"
S10	S10-2	PULL BOX PB06	SPARE	SPARE	1 3"	SPARE		
S11	S11-1	PULL BOX PB02	PULL BOX PB15	1 B SW	1 4"	12C#14, 7C#6	1 B SW	Typical for 4 4" (1 Spare)
S11	S11-2	PULL BOX PB02	PULL BOX PB15	N SIGNALS/N TRACKS	1 4"	2 7C #14/4 #6 TP	N SIGNALS	
S11	S11-3	PULL BOX PB02	PULL BOX PB15	3 SWITCHES	1 4"	2 12C#14, 2 7C#6	3 SWITCHES	
S11	S11-4	PULL BOX PB02	PULL BOX PB15	SPARE	1 4"	SPARE		
S12	S12-1	PULL BOX PB15	JB1B	1 B SW	1 4"	12C#14, 7C#6	1 B SW	TYPICAL FOR 2 4"
S12	S12-2	PULL BOX PB15	PULL BOX PB16	SPARE	1 4"	SPARE		
S13	S13-1	PULL BOX PB15	PULL BOX PB18	N SIGNALS/N TRACKS	1 4"	2 7C #14/4 #6 TP	N SIGNALS	Typical for 3 4"
S13	S13-2	PULL BOX PB15	PULL BOX PB18	3 SWITCHES	1 4"	2 12C#14, 2 7C#6	3 SWITCHES	
S13	S13-3	PULL BOX PB15	PULL BOX PB18	SPARE	1 4"	SPARE		
S14	S14-1	PULL BOX PB18	JB3B	3B SW	1 4"	12C#14, 7C#6	3 B SW	TYPICAL FOR 2 4"
S14	S14-2	PULL BOX PB18	PULL BOX PB19	SPARE	1 4"	SPARE		
S15	S15-1	PULL BOX PB18	PULL BOX PB20	N SIGNALS/N TRACKS	1 4"	2 7C #14/4 #6 TP	N SIGNALS	Typical for 3 4"
S15	S15-2	PULL BOX PB18	PULL BOX PB20	3A SW	1 4"	12C#14, 7C#6	3A SW	
S15	S15-3	PULL BOX PB18	PULL BOX PB20	SPARE	1 4"	SPARE		
S19	S19-1	PULL BOX PB20	JB3A	3A SW	1 4"	12C#14, 7C#6	3A SW	TYPICAL FOR 2 4"
S19	S19-2	PULL BOX PB20	PULL BOX PB25	SPARE	1 4"	SPARE		
S16	S16-1	PULL BOX PB20	PULL BOX PB21	N SIGNALS/N TRACKS	1 4"	2 7C #14/4 #6 TP	N SIGNALS	TYPICAL FOR 2 4"
S16	S16-2	PULL BOX PB20	PULL BOX PB21	SPARE	1 4"	SPARE		
S17	S17-1	PULL BOX PB21	PULL BOX PB22	N SIGNALS/N TRACKS	1 4"	2 7C #14/4 #6 TP	N SIGNALS	TYPICAL FOR 2 4"
S17	S17-2	PULL BOX PB21	PULL BOX PB22	SPARE	1 4"	SPARE		
S20	S20-1	PULL BOX PB22	2N	2N SIGNAL	1 4"	7C #14	2N SIGNAL	TYPICAL FOR 2 4"
S20	S20-2	PULL BOX PB22	1T/1NT	1NT	1 4"	2 #6 TP	10S/1NT	
S18	S18-1	PULL BOX PB22	PULL BOX PB23	4N/2T	1 4"	7C #14 2 #6 TP	4N/2T	TYPICAL FOR 2 4"
S18	S18-2	PULL BOX PB22	PULL BOX PB23	SPARE	1 4"	SPARE		
S21	S21-1	PULL BOX PB23	PULL BOX PB24	2T/2NT	1 4"	2 #6 TP	2T/2NT	TYPICAL FOR 3 4"

CONTROL POINT/CROSSOVERS			
FUNCTION	CABLE DETAIL	NAMING CONVENTION	FROM HOUSE TO:
4"	SF SWITCH	12C#14, 7C#6	1A SWITCH 1A
4"	CSV24 SWITCH	12C#14, 7C#6	1B SWITCH 1B
4"	CTS-2 SWITCH	12C#14, 7C#6	3A SWITCH 3A
4"	M23 SWITCH	12C#14, 7C#6	3B SWITCH 3B
4"	SIGNAL	7C#14	4N SIGNAL 4N
4"	SIGNAL	7C#14	2N SIGNAL 2N
4"	SIGNAL	7C#14	65 SIGNAL 65
4"	SIGNAL	7C#14	85 SIGNAL 85
4"	TRACK CIRCUIT	2C#6 TP	1NT NORTH T1
4"	TRACK CIRCUIT	2C#6 TP	1TR NORTH OS T1
4"	TRACK CIRCUIT	2C#6 TP	2NT NORTH T2
4"	TRACK CIRCUIT	2C#6 TP	2TR NORTH OS T2
4"	TRACK CIRCUIT	2C#6 TP	1TB SOUTH OS T1
4"	TRACK CIRCUIT	2C#6 TP	1ST SOUTH T1
4"	TRACK CIRCUIT	2C#6 TP	2TB SOUTH OS T2
4"	TRACK CIRCUIT	2C#6 TP	2ST SOUTH T2
3"	SIGNAL (SWITCH HEATER)	12C#14, 3C#2	HEATER CONTROLLER
SWITCH PAD			
4"	SWITCH MACHINE	7C#6	S1 Switch 1
4"	SWITCH MACHINE	7C#6	S2 Switch 2
4"	SWITCH MACHINE	7C#6, 12C#14	S3 Switch 3
4"	SWITCH MACHINE	7C#6, 12C#14	S4 Switch 4
CROSSING			
4"	GATE MECH	7C#6, 7C#14	XG1 NW CORNER
4"	GATE MECH	7C#6, 7C#14	XG2 SW CORNER
4"	GATE MECH	7C#6, 7C#14	XG3 SE CORNER
2"	TRAFFIC LOOPS	6 PR #18	TL CROSSING APRONS
4"	TRACK CIRCUIT	2C#6 TP	XT N OF CROSSING
4"	TRACK CIRCUIT	2C#6 TP	XR S OF CROSSING

S21	S21-2	PULL BOX PB23	4N SIGNAL	4N SIGNAL	1	4"	7C #14	4N SIGNAL	
S21	S21-3	PULL BOX PB23	SPARE	SPARE	1	4"		SPARE	
S22	S22-1	SIGNAL HOUSE	PULL BOX PB26	SW 1/2	1	4"	12C#14,7C#6,7C #14	SW 1/2	Typical for 6 4" & 2 2"
S22	S22-2	SIGNAL HOUSE	PULL BOX PB26	SW 3/4	1	4"	12C#14,7C#6,7C #14	SW 3/4	
S22	S22-3	SIGNAL HOUSE	PULL BOX PB26	XG1/XT/XR/1T	1	4"	7C#6, 7C#14 3 #6 TP	XG1	
S22	S22-4	SIGNAL HOUSE	PULL BOX PB26	XG2/LOOPS	1	4"	7C#6, 7C#14/2 2C TP	XG2	
S22	S22-5	SIGNAL HOUSE	PULL BOX PB26	XG3/NT	1	4"	7C#6, 7C#14, 2 #6 TP	XG3/Indicator	
S22	S22-6	SIGNAL HOUSE	PULL BOX PB26	SPARE	1	4"		SPARE	
S22	S22-7	SIGNAL HOUSE	PULL BOX PB26	FO1/FO2 FIBER	1	2"	F24 SINGLE MODE FIBER	FO1	
S23	C22-2	SIGNAL HOUSE	PULL BOX PB26	SPARE	1	2"		SPARE	
S23	S23-1	PULL BOX PB26	PULL BOX PB27	XG1/XT/XR/1T	1	4"	7C#6, 7C#14 3 #6 TP	XG1	Typical for 4 4" & 2 2"
S23	S23-2	PULL BOX PB26	PULL BOX PB27	XG2/LOOPS	1	4"	7C#6, 7C#14/2 2C TP	XG2	
S23	S23-3	PULL BOX PB26	PULL BOX PB27	XG3/NT	1	4"	7C#6, 7C#14, 2 #6 TP	XG3/Indicator	
S23	S23-4	PULL BOX PB26	PULL BOX PB27	SPARE	1	4"		SPARE	
S23	C23-1	PULL BOX PB26	PULL BOX PB27	FO1/FO2 FIBER	1	2"	F24 SINGLE MODE FIBER	FO2	
S23	C23-2	PULL BOX PB26	PULL BOX PB27	SPARE	1	2"		SPARE	
S24	SC24-1	PULL BOX PB27	PULL BOX PB28	XG1/XT/XR	1	4"	7C#6, 7C#14 2 #6 TP	XG1	Typical for 4 4" & 2 2"
S24	SC24-2	PULL BOX PB27	PULL BOX PB28	XG2/LOOPS	1	4"	7C#6, 7C#14/2 2C TP	XG2	
S24	SC24-3	PULL BOX PB27	PULL BOX PB28	XG3	1	4"	7C#6, 7C#14, 2 #6 TP	XG3/Indicator	
S24	SC24-4	PULL BOX PB27	PULL BOX PB28	SPARE	1	4"		SPARE	
S24	C24-1	PULL BOX PB27	TRAINING BUILDING	FO1/FO2 FIBER	1	2"	F24 SINGLE MODE FIBER	FO1/FO2	
S24	C24-2	PULL BOX PB27	TRAINING BUILDING	SPARE	1	2"		SPARE	
S26	S26-1	PULL BOX PB28	PULL BOX PB30	XG1/XT/XR	1	4"	7C#6, 7C#14 2 #6 TP	XG3/Indicator	TYPICAL FOR 2 4" & 2 3"
S26	S26-2	PULL BOX PB28	PULL BOX PB30	SPARE	1	4"		SPARE	
S26	S26-3	PULL BOX PB28	PULL BOX PB30	LOOPS	1	3"	2C TP	LOOPS	
S26	S26-4	PULL BOX PB28	PULL BOX PB30	SPARE	1	3"		SPARE	
S26.5	S26.5-1	PULL BOX PB30	XR	XR	1	3"	2C #6 TP	XR TAPS	TYPICAL FOR 2 3" CONDUIT
S26.5	S26.5-2	PULL BOX PB30	XR	SPARE	1	3"		SPARE	
S27	S27-1	PULL BOX PB30	PULL BOX PB31	XG1/XT	1	4"	7C#6, 7C#14, 2C #6 TP	XG1	TYPICAL FOR 2 3" CONDUIT
S27	S27-2	PULL BOX PB30	PULL BOX PB31	SPARE	1	4"		SPARE	
S29	S29-1	PULL BOX PB31	XG1	XG1	1	3"	7C#6, 7C#14	XG1	TYPICAL for 3 4"
S29	S29-2	PULL BOX PB31	XT	XT	1	3"	2C #6 TP	XT TAPS	
S29	S29-3	PULL BOX PB31	SPARE	SPARE	1	3"		SPARE	
S28	S28-1	PULL BOX PB31	PULL BOX PB32	LOOPS	1	3"	2C TP	LOOPS	TYPICAL FOR 2 3"
S28	S28-2	PULL BOX PB31	PULL BOX PB32	SPARE	1	3"		SPARE	
S30	C30-1	PULL BOX PB27	PULL BOX PB39	FO1/FO2 FIBER	1	2"	2 F24 SINGLE MODE FIBER	FO1/FO2	TYPICAL FOR 2 2" & 3 3"
S30	C30-2	PULL BOX PB27	PULL BOX PB39	SPARE	1	2"		SPARE	
S30	S30-1	PULL BOX PB27	PULL BOX PB39	1T	1	3"	2C #6 TP	1T	
S30	S30-2	PULL BOX PB27	PULL BOX PB39	SPARE	1	3"		SPARE	
S31	C31-1	PULL BOX PB39	PULL BOX PB40	FO1/FO2 FIBER	1	2"	2 F24 SINGLE MODE FIBER	FO1/FO2	TYPICAL FOR 2 2" & 3 3"
S31	C31-2	PULL BOX PB39	PULL BOX PB40	SPARE	1	2"		SPARE	
S31	S31-1	PULL BOX PB39	PULL BOX PB40	1T	1	3"	2C #6 TP	1T	
S31	S31-2	PULL BOX PB39	PULL BOX PB40	SPARE	1	3"		SPARE	
S32	C32-1	PULL BOX PB40	PULL BOX PB41	FO1/FO2 FIBER	1	2"	2 F24 SINGLE MODE FIBER	FO1/FO2	TYPICAL FOR 2 2" & 3 3"
S32	C32-2	PULL BOX PB40	PULL BOX PB41	SPARE	1	2"		SPARE	
S32	S32-1	PULL BOX PB40	PULL BOX PB41	1T	1	3"	2C #6 TP	1T	
S32	S32-2	PULL BOX PB40	PULL BOX PB41	SPARE	1	3"		SPARE	
S33	C33-1	PULL BOX PB41	PULL BOX PB42	FO1/FO2 FIBER	1	2"	2 F24 SINGLE MODE FIBER	FO1/FO2	TYPICAL FOR 2 2" & 3 3"
S33	C33-2	PULL BOX PB41	PULL BOX PB42	SPARE	1	2"		SPARE	
S33	S33-1	PULL BOX PB41	PULL BOX PB42	1T	1	3"	2C #6 TP	1T	
S33	S33-2	PULL BOX PB41	PULL BOX PB42	SPARE	1	3"		SPARE	
S34	S34-1	PULL BOX PB42	PULL BOX PB43	1T	1	2"	2C #6 TP	1T	TYPICAL FOR 2 2" & 3 3"
S34	S34-2	PULL BOX PB42	PULL BOX PB43	SPARE	1	2"		SPARE	
S34	C34-1	PULL BOX PB42	PULL BOX PB43	FO1/FO2 FIBER	1	3"	2 F24 SINGLE MODE FIBER	FO1/FO2	
S34	C34-2	PULL BOX PB42	PULL BOX PB43	SPARE	1	3"		SPARE	
S35	C35-1	PULL BOX 43	PULL BOX 46	FO1/FO2 FIBER	1	3"	2 F24 SINGLE MODE FIBER	FO1/FO2	TYPICAL FOR 2 3"
S35	C35-2	PULL BOX 43	PULL BOX 46	SPARE	1	3"		SPARE	
S36	C36-1	PULL BOX 46	FIBER FEED	FO1/FO2 FIBER	1	3"	2 F24 SINGLE MODE FIBER	FO1/FO2	TYPICAL FOR 2 3"
S36	C36-2	PULL BOX 46	FIBER FEED	SPARE	1	3"		SPARE	

- LEGEND
- S=SIGNS
  - C=COMMS
  - P=POWER
  - O=OCS
  - L=LIGHTING
  - DB=DUCT BANK
  - JB=JUNCT. BOX

CABLE CONSTRUCTION								
POWER CONDUIT ID	CONDUIT #	FROM	TO	SIZE [INCHES]	TYPE	CIRCUIT NAME	SIZE (AWG/KCMIL)	BANK TYPE
P01	P01-1	SIGNAL HOUSE	FEED	4"	SCH 80		TBD	Typical for 4x4"
	P01-2			4"	SCH 80		TBD	
	P01-3			4"	SCH 80		TBD	
	P01-4			4"	SCH 80		TBD	
P02	P02-1	FEED	TPSS	4"	SCH 80		TBD	Typical for 4x4"
	P02-2			4"	SCH 80		TBD	
	P02-3			4"	SCH 80		TBD	
	P02-4			4"	SCH 80		TBD	
P03	P03-1	FEED	PULL BOX PB35	6"	SCH 80		TBD	Typical for 4x6"
	P03-2			6"	SCH 80		TBD	
	P03-3			6"	SCH 80		TBD	
	P03-4			6"	SCH 80		TBD	
P04	P04-1	PULL BOX PB35	PULL BOX PB36	6"	SCH 80		TBD	Typical for 4x6"
	P04-2			6"	SCH 80		TBD	
	P04-3			6"	SCH 80		TBD	
	P04-4			6"	SCH 80		TBD	
P05	P05-1	PULL BOX PB36	PULL BOX PB37	6"	SCH 80		TBD	Typical for 4x6"
	P05-2			6"	SCH 80		TBD	
	P05-3			6"	SCH 80		TBD	
	P05-4			6"	SCH 80		TBD	
P06	P06-1	PULL BOX PB37	PULL BOX PB38	6"	SCH 80		TBD	Typical for 4x6"
	P06-2			6"	SCH 80		TBD	
	P06-3			6"	SCH 80		TBD	
	P06-4			6"	SCH 80		TBD	
P07	P07-1	PULL BOX PB38	METER	6"	SCH 80		TBD	Typical for 4x6"
	P07-2			6"	SCH 80		TBD	
	P07-3			6"	SCH 80		TBD	
	P07-4			6"	SCH 80		TBD	

LEGEND

S=SIGNSL
C=COMMS
P=POWER
O=OCS
L=LIGHTING
DB=DUCTBANK
JB=JUNCT. BOX

OCS and LIGHTING SCHEDULE								
DUCTBANK#	CONDUIT #	FROM	TO	DESTINATION	CONDUIT SIZE	CONDUIT CONTENTS	CIRCUIT NAME	BANK TYPE
O/L01	O/L01-1	TPSS	SWITCH GEAR	LIGHTING	1 4"	2 3C #2	LIGHTING TRAX LF1, LF3	TYPICAL FOR 8 4"
	O/L01-2			S NB FEED	1 4"	1-500 KCMIL	DS-1B	
	O/L01-3			S SB FEED	1 4"	1-500 KCMIL	DS-1A	
	O/L01-4			N NB FEED	1 4"	1-500 KCMIL	DS-2B	
	O/L01-5			N SB FEED	1 4"	1-500 KCMIL	DS-2A	
	O/L01-6			LV CIRCUITS (1/2)	1 4"	12-2/C #14 (SCADA) 4-2/C #14 (Voltage Transducer)	DISC. SW. CONTROL (1/2)	
	O/L01-7			LV CIRCUITS (2/2)	1 4"	1-2/C #10 (Control Power) 1-1/C #6 (Neg Ref)	DISC. SW. CONTROL (2/2)	
	O/L01-8			SPARE	1 4"		SPARE	
O/L04	O/L04-1	SWITCH GEAR	PULL BOX PB15	LIGHTING	1 4"	2 3C #2	LIGHTING TRAX LF1, LF3	TYPICAL FOR 8 4"
	O/L04-2			S NB FEED	1 4"	1-500 KCMIL	DS-1B	
	O/L04-3			S SB FEED	1 4"	1-500 KCMIL	DS-1A	
	O/L04-4			N NB FEED	1 4"	1-500 KCMIL	DS-2B	
	O/L04-5			N SB FEED	1 4"	1-500 KCMIL	DS-2A	
	O/L04-6			SPARE	1 4"		SPARE	
	O/L04-7			SPARE	1 4"		SPARE	
	O/L04-8			SPARE	1 4"		SPARE	
O/L03	O/L03-1	PULL BOX PB15	PULL BOX PB03	LIGHTING	1 4"	3C #2	LIGHTING TRAX LF1	TYPICAL FOR 4 4"
	O/L03-2			S NB FEED	1 4"	1-500 KCMIL	DS-1B	
	O/L03-3			S SB FEED	1 4"	1-500 KCMIL	DS-1A	
	O/L03-4			SPARE	1 4"		SPARE	
O/L05.5	O/L05.5-1	PULL BOX PB03	T1 SOUTH FEED	S NB FEED	1 4"	1-500 KCMIL	DS-1B	TYPICAL FOR 2 4"
	O/L05.5-2			SPARE	1 4"		SPARE	
O/L07	O/L07-1	PULL BOX PB03	1A SOUTH SB FEED	LIGHTING	1 4"	3C #2	LIGHTING TRAX LF1	TYPICAL FOR 3 4"
	O/L07-2			S SB FEED	1 4"	1-500 KCMIL	DS-1A	
	O/L07-3			SPARE	1 4"		SPARE	
O/L02	O/L02-1	PULL BOX PB14	PULL BOX PB17	LIGHTING	1 4"	3C #2	LIGHTING TRAX LF2	TYPICAL FOR 3 4"
	O/L02-2			NEG RETURN	1 4"	2-500 KCMIL	NEG	
	O/L02-3			SPARE	1 4"		SPARE	
O/L08	O/L08-2	PULL BOX PB14	PULL BOX PB17	NEG RETURN	1 4"	2-500 KCMIL	NEG	TYPICAL FOR 2 4"
	O/L08-3			SPARE	1 4"		SPARE	
O/L09	O/L09-1	PULL BOX PB15.5	PULL BOX PB020	LIGHTING	1 4"	3C #2	LIGHTING TRAX LF3	TYPICAL FOR 4 4"
	O/L09-2			N NB FEED	1 4"	1-500 KCMIL	DS-2B	
	O/L09-3			N SB FEED	1 4"	1-500 KCMIL	DS-2A	
	O/L09-4			SPARE	1 4"		SPARE	
O/L08	O/L8-1	PULL BOX PB14	NEG RETURN TAPS	NEG RETURN	1 4"	2-500 KCMIL	NEG	TYPICAL FOR 2 4"
	O/L8-2			SPARE	1 4"		SPARE	
O/L10	O/L10-1	PULL BOX PB20	2A & 2B NORTH NB & SB FEEDS	LIGHTING	1 4"	3C #2	LIGHTING TRAX LF3	TYPICAL FOR 4 4"
	O/L10-2			N NB FEED	1 4"	1-500 KCMIL	DS-2B	
	O/L10-3			N SB FEED	1 4"	1-500 KCMIL	DS-2A	
	O/L10-4			SPARE	1 4"		SPARE	
O/L12	O/L12-1	FEED	TPSS	LIGHTING	1 4"	2 3C #2	LIGHTING TRAX LF1, LF3	TYPICAL FOR 3 4"
	O/L12-2			LIGHTING	1 4"	3C #2	LIGHTING TRAX LF2	
	O/L12-3			SPARE	1 4"		SPARE	
O/L14	O/L14-1	PULL BOX PB33	PULL BOX PB26	LIGHTING	1 4"	3C #2	LIGHTING FRONTRUNNER LF4,5,6	TYPICAL FOR 2 4"
	O/L14-2			SPARE	1 4"		SPARE	
O/L15	O/L15-1	PULL BOX PB26	PULL BOX PB27	LIGHTING	1 4"	3C #2	LIGHTING FRONTRUNNER LF4,5,6	TYPICAL FOR 2 4"
	O/L15-2			SPARE	1 4"		SPARE	
O/L16	O/L16-1	PULL BOX PB27	PULL BOX PB39	LIGHTING	1 4"	3C #2	LIGHTING FRONTRUNNER LF4,5,6	TYPICAL FOR 2 4"
	O/L16-2			SPARE	1 4"		SPARE	
O/L17	O/L17-1	PULL BOX PB39	PULL BOX PB40	LIGHTING	1 4"	3C #2	LIGHTING FRONTRUNNER LF5,6	TYPICAL FOR 2 4"
	O/L17-2			SPARE	1 4"		SPARE	
O/L18	O/L18-1	PULL BOX PB40	PULL BOX PB41	LIGHTING	1 4"	3C #2	LIGHTING FRONTRUNNER LF5,6	TYPICAL FOR 2 4"
	O/L18-2			SPARE	1 4"		SPARE	
O/L19	O/L19-1	PULL BOX PB41	PULL BOX PB42	LIGHTING	1 4"	3C #2	LIGHTING FRONTRUNNER LF6	TYPICAL FOR 2 4"
	O/L19-2			SPARE	1 4"		SPARE	
O/L20	O/L20-1	PULL BOX PB42	PULL BOX PB43	LIGHTING	1 4"	3C #2	LIGHTING FRONTRUNNER LF6	TYPICAL FOR 2 4"
	O/L20-2			SPARE	1 4"		SPARE	

LEGEND

S=SIGNSL
C=COMMS
P=POWER
O=OCS
L=LIGHTING
DB=DUCTBANK
JB=JUNCT. BOX

